

Indiana Department of Transportation

County Delaware Route Nebo Road Des. No. 1700680

FHWA-Indiana Environmental Document CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM GENERAL PROJECT INFORMATION

Road No./County:

Nebo Road/Delaware County

Designation Number:

1700680

Project Description/Termini:

The project involves the replacement of Delaware County Bridge No. 125 (Bridge No. 18-00125) on Nebo Road over York Prairie Creek from 0.45 mile north of River Road to 0.61 mile north of River Road.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

X	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

ESM Signature

Date

ES Signature

Date

FHWA Signature

Date

Release for Public Involvement

N/A

ESM Initials

Date



ES Initials

7-19-2020

Date

Certification of Public Involvement

Office of Public Involvement

Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env.

Reviewer Signature: _____

Date: _____

Name and Organization of CE/EA

Preparer: _____

Chris Kunkel/Lochmueller Group

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*? Yes ☐ No ☒
If No, then: ☒ ☐
Opportunity for a Public Hearing Required?

**A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

Notice of Entry

Notice of Entry letters were mailed to potentially affected property owners within the project area on April 30, 2019 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, G1 to G2.

Public Involvement:

The project will meet the minimum requirements described in the current Indiana Department of Transportation (INDOT) *Public Involvement Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental GroundsWill the project involve substantial controversy concerning community and/or natural resource impacts? Yes ☐ No ☒

Remarks:

At this time, there is no substantial public controversy concerning impacts to the community or to natural resource.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Delaware County INDOT District: Greenfield
Local Name of the Facility: Nebo RoadFunding Source (mark all that apply): Federal ☒ State ☐ Local ☒ Other* ☐*If other is selected, please identify the funding source: N/A

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PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

Need:

The need for the project stems from the deteriorated condition and the insufficient hydraulics of the existing bridge (Bridge No. 18-00125). As described in the most recent INDOT *Bridge Inspection Report* from November 2018 (excerpts from this report are included in Appendix J, J2 to J10), the condition ratings of the wearing surface, deck, and channel are all 5, which indicates "fair" condition. The condition rating of the superstructure is a 4, which indicates "poor" condition. The condition rating of the substructure is a 3, which indicates "serious" condition. The overall condition rating of the bridge is a 3. Condition ratings range from 0 to 9, with 0 meaning a failed component and 9 indicating a new component with no deficiencies. There is also minimal clearance between the 100-year floodplain elevation and the underside of the existing bridge. This clearance is known as freeboard. According to the October 2019 *Hydraulic Report* performed by Lochmueller Group, the freeboard of the existing bridge is 4 inches. According to the *Indiana Design Manual*, a bridge structure should have a minimum of 2 feet of freeboard, where practical. The remaining service life of the bridge is approximately 10 years.

Purpose:

The purpose of the project is to have a structure that achieves a condition rating of at least 8, which is considered "very good" condition. Additionally, the improved crossing should provide a service life of at least 50 years total and provide over 2 feet of freeboard between the bottom of the structure and 100-year floodplain elevation.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: DelawareMunicipality: YorktownLimits of Proposed Work: From 0.45 mile north of River Road to 0.61 mile north of River Road along Nebo RoadTotal Work Length: 0.16 Mile(s)Total Work Area: 1.66 Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?

If yes, when did the FHWA grant a conditional approval for this project?

Yes¹

No

☐☒

Date:

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

The Federal Highway Administration (FHWA) and the Delaware County Board of Commissioners intend to proceed with a federal-aid project to replace the existing bridge, Bridge No. 125 (National Bridge Inventory (NBI) No. 1800105) that carries Nebo Road over York Prairie Creek in Yorktown, Delaware County, Indiana.

Location:

The project is located in west-central Delaware County along Nebo Road, 0.50 mile north of CR 50 South. Specifically, the project is located in Mt. Pleasant Township within Sections 11 and 12, Township 20 North, Range 9 East, as depicted on the Muncie West, Indiana USGS 7.5-minute Quadrangle (Appendix B, B2).

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Existing Conditions:

Nebo Road is functionally classified as an urban minor arterial within the project area and consists of two 11-foot wide asphalt travel lanes (one in each direction) with a 1-foot wide paved shoulder and a variable width gravel shoulder on both sides. The posted speed limit within the project area is 40 miles per hour (mph).

Bridge No. 125 is a 67-foot long concrete continuous slab bridge widened with steel beams with a 34.6-foot wide out-to-out deck width that carries Nebo Road over York Prairie Creek. The existing bridge has 68 feet of steel bridge railing along both sides. Additionally, there is guardrail along the north and south approaches of the bridge on either side of Nebo Road. The structure was built in 1954 and reconstructed in 1975. The typical section of Nebo Road on structure consists of two 11-foot wide travel lanes with a 5-foot wide shoulder on both sides. Two corrugated metal pipes (CMP), one with a 12-inch diameter and one with a 6-inch diameter, are present in the northeast quadrant of the bridge. Both structures convey drainage from the northeast quadrant into York Prairie Creek.

Several components of Bridge 125 were noted to have deficiencies. Gaps have formed between the roadway surface and both the east and west coping of the bridge. Spalls were also observed on the deck. There is widespread cracking on the wearing surface and debris was observed in the joints. On the superstructure, there is widespread rust and deterioration on the steel beams and widespread spalling on the underside of the deck. On the substructure, the piers have large vertical cracks and there is heavy spalling. Additionally, the concrete slope walls have been undermined and the soil beneath them is beginning to erode. The bank of York Prairie Creek is heavily eroded. Please refer to the INDOT Bridge Inspection Report in Appendix J, pages J2 to J10.

Adjacent land use within the project area includes recreational, residential, agricultural, and forested land (Appendix B, B3).

Preferred Alternative:

The preferred alternative will replace the existing bridge with a single-span, composite, prestressed concrete box beam bridge with a 60-foot span. The new structure will have a length of 63.67 feet and an out-to-out deck width of 34.6 feet wide. The roadway profile will be raised approximately 2.5 feet across the bridge to provide adequate hydraulics for the new structure. On structure, Nebo Road will consist of two 11-foot wide travel lanes with 2-foot shoulders on both sides. Additionally, a 6-foot sidewalk with a 7-inch curb will be constructed along the east side of the bridge. The inclusion of the sidewalk on the structure provides sufficient width for future multi-modal connectivity. The existing steel bridge railing will be replaced by a concrete and steel bridge railing. The existing guardrail along both sides of Nebo Road will be replaced with new standard guardrail. Riprap will be placed along the spill slopes.

The approach roadway, from approximately 413 feet north and 281 feet south of the structure, will be replaced to full depth and raised to accommodate the gradual 2.5-foot raise in profile at the bridge. Additionally, 42.5 feet south and 50 feet north of the full-depth approach replacement, Nebo Road will be milled to a depth of 1.5 inches and a new 1.5-inch hot mix asphalt overlay applied atop the milled surface. The typical section of reconstructed Nebo Road will consist of two 11-foot wide asphalt travel lanes (one in each direction) with a 2.6-foot curb and gutter on either side.

The existing CMP structures in the northeast quadrant of the bridge will be removed as part of the project. New stormwater drainage will be installed in place of the removed pipes. This will consist of storm water inlets connecting to 12-inch pipes added beneath the curb on either side of Nebo Road. One pipe will begin approximately 284 feet north of the bridge structure at a drainage inlet and flow south and outlet into York Prairie Creek. The other pipe will begin approximately 269 feet south of the bridge structure at a drainage inlet and outlet into York Prairie Creek. The length of channel work along York Prairie Creek will be approximately 172 linear feet. The total project length along Nebo Road is 850 feet (Appendix B, B17).

This project will meet the purpose and need of the project as replacing the entire bridge with a new structure will provide a new structure having a condition rating of at least an 8. Additionally, the new structure will have a service life of approximately 75 years and the freeboard of the new structure will be over 2 feet.

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The termini were established because they are the logical limits required to replace the bridge structure, install new riprap, replace the guardrail, and to match the grade of the approach roadway to the new bridge. This project will be completed independent of any other project and is able to be constructed without relying on the completion of any other project.

Maintenance of Traffic (MOT):

The proposed MOT plan includes the closure of Nebo Road during construction (Appendix B, B16). A detour utilizing Jackson Street, Morison Road, and W River Road will be established. Please refer to the *Maintenance of Traffic* section of this document for full details. The MOT will be implemented per the *Indiana Design Manual* guidelines.

Right-of-Way

The proposed project will require the acquisition of 0.89 acre of permanent right-of-way (ROW) (Appendix B, B3). No relocations will be required. Approximately 0.23 acre of tree clearing within the construction limits will be required for this project.

OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

Bridge Rehabilitation: This alternative would involve repairing the specific components of the bridge with patching, surface sealing, and bridge deck overlay activities. This work could possibly be done without closing Nebo Road. While the impacts to environmental resources and the overall footprint would be reduced and the cost would likely be less compared to the preferred alternative, the project would not meet the purpose and need. The current bridge does not meet current design standards for freeboard. It is unknown what the expected lifespan of the bridge would be if this alternative was chosen but the freeboard would still be approximately 4 inches. By not raising the profile of the bridge, current design standards would not be met. Therefore, this alternative was discarded from further consideration.

Do Nothing Alternative: This alternative involved not improving Bridge 125. While this alternative eliminates costs and any environmental impacts, it would not have met the objectives of the purpose and need of the project. Therefore, this alternative was discarded from further consideration.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

It would not correct existing capacity deficiencies;
It would not correct existing safety hazards;
It would not correct the existing roadway geometric deficiencies;
It would not correct existing deteriorated conditions and maintenance problems; or
It would result in serious impacts to the motoring public and general welfare of the economy.
Other (Describe)

X

ROADWAY CHARACTER:

Nebo Road:

Functional Classification:	Minor Arterial				
Current ADT:	10,003	VPD (2021)	Design Year ADT:	14,865	VPD (2041)
Design Hour Volume (DHV):	856	Truck Percentage (%)	5		
Designed Speed (mph):	40	Legal Speed (mph):	40		

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Existing

Proposed

Number of Lanes:	2		2	
Type of Lanes:	Travel lanes		Travel lanes	
Pavement Width:	24	ft.	26	ft.
Shoulder Width:	1	ft.	2.6	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting: ☐ Urban ☒ Suburban ☐ Rural
 Topography: ☒ Level ☐ Rolling ☐ Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

DESIGN CRITERIA FOR BRIDGES:

Structure/NBI Number(s): Local: 18-00125/ Sufficiency Rating: 48.3 (November 2018, Bridge
NBI: 1800105 Inspection)
 (Rating, Source of Information)

Existing

Proposed

Bridge Type:	Reinforced Concrete Slab		Concrete Box Beam	
Number of Spans:	3		1	
Weight Restrictions:	N/A	ton	N/A	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	34	ft.	26	ft.
Outside to Outside Width:	34.6	ft.	34.6	ft.
Shoulder Width:	5	ft.	2	ft.
Length of Channel Work:			172	ft.

Describe bridges and structures; provide specific location information for small structures.

Remarks:

The proposed project will involve the replacement of Bridge No. 125, which carries Nebo Road over York Prairie Creek. The proposed project will impact a total of 172 linear feet of York Prairie Creek (Appendix B, B3). See the *Project Description* section of this document for more details on the work that will occur to this bridge.

Two CMPs, a 6-inch diameter and a 12-inch diameter, located in the northeast quadrant of the bridge will also be removed. There will also be the addition of four drainage pipes. Locations and dimensions of these pipes are detailed below.

Structure No.	Type	Size (length by diameter)	Location
100	Inlet with drainage pipe (under Nebo Road)	24 feet by 12 inches	SE quadrant (flows into Str. 101)
101	Inlet with drainage pipe	269 feet by 12 inches	SW quadrant (outlets into York Prairie Creek)
102	Inlet with drainage pipe	284 feet by 12 inches	NW quadrant (outlets into York Prairie Creek)
103	Inlet with drainage pipe (under Nebo Road)	24 feet by 12 inches	NE quadrant (flows into Str. 102)

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Will the structure be rehabilitated or replaced as part of the project? **Yes**
☒ **No**
☐ **N/A**
☐
If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: The MOT will require the closure of Nebo Road during construction (Appendix B, B15). A detour utilizing River Road, Morrison Road, and Jackson Street will be established. The detour be an added travel length of approximately 4.3 miles. Access to all drives and businesses will be maintained during construction. Standard INDOT signage and barricades will be posted to inform motorists of the road closure and detour route during construction. The detour is expected to last approximately six months. The MOT will be implemented per the *Indiana Design Manual* guidelines.

The closure will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences will cease upon project completion.

Early coordination information was sent to Yorktown Community Schools, Delaware County Board of Commissioners, Delaware County Council, Delaware County Highway Department, Delaware County Emergency Medical Service, Delaware County Sheriff's Department, Yorktown Fire Department, and Yorktown Police Department on February 5, 2020 (Appendix C, C1 to C5). None of the agencies respond to the early coordination letter. The contractor will be responsible for contacting school districts and emergency services in accordance with current INDOT *Design Manual* and *Standard Specifications*.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 299,400 (2019) Right-of-Way: \$ 65,000 (2020) Construction: \$ 1,625,000 (2022)

Anticipated Start Date of Construction: Spring 2022

Date project incorporated into STIP July 2, 2019

Is the project in an MPO Area? **Yes**
☒ **No**
☐

If yes,

Name of MPO Delaware-Muncie Metropolitan Plan Commission

Location of Project in TIP Page 33

Date of incorporation by reference into the STIP January 24, 2020

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RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.15	0.00
Commercial	0.00	0.00
Agricultural	0.15	0.00
Forest	*0.34	0.00
Wetlands	0.00	0.00
Other: Recreational (Golf Course)	0.25	0.00
Other:	0.00	0.00
TOTAL	0.89	0.00

*0.26 acre of forested land use is considered prime farmland by the National Resource Conservation Service (NRCS).

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

Remarks:

Within the project area, the apparent existing ROW along Nebo Road varies from 62 feet to 80 feet wide (31 feet to 40 feet from the roadway centerline). There is also an existing drainage easement that is 75 feet wide from the top of either bank of York Prairie Creek.

The project requires approximately 0.89 acre of permanent ROW from residential (0.15 acre), recreational (0.25 acre), agricultural (0.15 acre), and forested (0.34 acre) land. The new ROW will have a typical width of 105 feet wide (50 feet west and 55 feet east of the roadway centerline) and a maximum width of 155 feet wide (75 feet west and 80 feet east of the roadway centerline) at the bridge (Appendix B, B3 and B17).

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

Streams, Rivers, Watercourses & Jurisdictional Ditches

Federal Wild and Scenic Rivers
State Natural, Scenic or Recreational Rivers
Nationwide Rivers Inventory (NRI) listed
Outstanding Rivers List for Indiana
Navigable Waterways

Presence

X

Impacts

Yes	No
X	

Remarks:

Based on a desktop review, a site visit on July 31, 2019 by Lochmueller Group, the 2017 aerial map of the project area (Appendix B, B3), the USGS topographic map (Appendix B, B2), and the water resources map of the Red Flag Investigation (RFI) report (Appendix E, E9) there are 2 streams located within the 0.5 mile search radius. There is one stream present within or adjacent to the project area.

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A *Waters of the U.S. Determination Report* was completed by Lochmueller Group on August 23, 2019. Please refer to Appendix F, F1 to F21 for the *Waters of the U.S. Determination Report*. It was determined that approximately 194 linear feet of one stream, York Prairie Creek, is located in the project area. York Prairie Creek flows southeast to northwest through the project area. York Prairie Creek is likely a Water of the U.S. due to the well-defined ordinary high-water mark (OHWM) and the hydrologic connection with the West Fork White River, a Traditionally Navigable Water. York Prairie Creek flows west to the West Fork White River, approximately 6.38 river miles downstream of the project area. According to the USGS (1:24,000 scale) topographic map, York Prairie Creek is a mapped state perennial stream. York Prairie Creek has an OHWM of 18 feet wide and 9 inches deep at this crossing. York Prairie Creek is not listed as a Federal Wild and Scenic River, a State Natural, Scenic, and Recreational River or as an Indiana Department of Natural Resources (IDNR) Outstanding River. The U.S. Army Corp of Engineers (USACE) makes all final determinations regarding jurisdiction.

York Prairie Creek is Indiana Department of Environmental Management (IDEM) 303d listed as impaired for *E. coli*. Workers who are working in or near water with *E. coli* should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure.

Approximately 172 feet (0.07 acre below OHWM) of York Prairie Creek will be impacted by the project. Impacts will be limited to the portion of the stream within the construction limits of the project. Impacts to the stream will include construction access activities, excavation to remove the existing bridge piers and install the new bridge components, and installing riprap along the spill slopes. Due to permanent impacts to a Water of the U.S., a USACE Section 404 Regional General Permit (RGP) and an IDEM Section 401 Water Quality Certification (WQC) will be required. Because impacts are below the threshold of 300 linear feet and 0.1 acre to require mitigation, it is not anticipated that mitigation will be required.

Early coordination information was sent on February 5, 2020 to the USACE and the IDNR Division of Fish and Wildlife (DFW) (Appendix C, C1 to C5). The USACE did not respond to the early coordination letter. The IDNR DFW responded on March 9, 2020 with recommendations to avoid or minimize impacts to York Prairie Creek (Appendix C, C45 to C48). Applicable recommendations include keeping the natural stream substrate after the new structure is constructed, vegetation establishment along the banks, the use and type of riprap for erosion control, time of year restrictions on work within the channel, and minimizing stream disturbance to be within construction limits.

An automated letter was generated from the Indiana Department of Environmental Management (IDEM) website on February 5, 2020 (Appendix C, C9 to C16). Applicable recommendations from the Proposed Roadway Letter include coordinating with appropriate agencies with regards to stream impacts and limiting stream disturbance.

All applicable agency recommendations are included in the *Environmental Commitments* section of this CE document.

Other Surface Waters

Reservoirs

Lakes

Farm Ponds

Detention Basins

Storm Water Management Facilities

Other: _____

Presence

Impacts

Yes	No

Remarks:

Based on a desktop review, a site visit on July 31, 2019 by Lochmueller Group, the 2017 aerial map of the project area (Appendix B, B3), the USGS topographic map (Appendix B, B2), and the water resource map in

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the RFI (Appendix E, E9), there are 5 other surface waters located within the 0.5 mile search radius. There are no other surface waters present within or adjacent to the project area.

A *Waters of the U.S. Determination Report* was completed for the project on August 23, 2019 by Lochmueller Group. Please refer to Appendix F, F1 to F21 for the *Waters of the U.S. Determination Report*. It was determined that there are no other surface water features within or near the project area. Therefore, no impacts are expected.

The USACE did not respond to the early coordination letter. IDNR DFW responded on March 9, 2020 and had no recommendations related to other surface waters (Appendix C, C45 to C48).

An automated letter was generated from the IDEM website on February 5, 2020 (Appendix C, C9 to C16). No recommendation relating to other surface waters are applicable to this project.

Presence **Impacts**
Yes No

Wetlands ☐ ☐ ☐

Total wetland area: N/A acre(s) Total wetland area impacted: N/A acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
N/A	N/A	N/A	N/A	N/A

Documentation

ES Approval Dates

Wetlands (Mark all that apply)

Wetland Determination
Wetland Delineation
USACE Isolated Waters Determination
Mitigation Plan

X

N/A

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;
Substantially increased project costs;
Unique engineering, traffic, maintenance, or safety problems;
Substantial adverse social, economic, or environmental impacts, or
The project not meeting the identified needs.

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks: Based on a review of the National Wetlands Inventory (NWI) on-line mapper (<https://www.fws.gov/wetlands/data/Mapper.html>) (Appendix F, F9), a site visit on July 31, 2019 by Lochmueller Group, the USGS topographic map (Appendix B, B2), and the water resource map of the RFI report (Appendix E, E9), there are 9 wetlands located within the 0.5 mile search radius. There are no wetlands present within or adjacent to the project area.

A *Waters of the U.S. Determination Report* was completed for the project on August 23, 2019 by Lochmueller Group. Please refer to Appendix F, F1 to F21 for the *Waters of the U.S. Determination Report*. It was determined that there are no wetlands within or adjacent to the project area. Therefore, no impacts are expected.

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The USACE did not respond to the early coordination letter. IDNR DFW responded on March 9, 2020 with recommendations to relating to wetland impacts (Appendix C, C45 to C48). Since no wetland impacts will occur, these recommendations do not apply.

An automated letter was generated from the IDEM website on February 5, 2020 (Appendix C, C9 to C16). No recommendations related to wetlands are applicable to this project.

	<u>Presence</u>	<u>Impacts</u>				
		Yes	No			
Terrestrial Habitat	<table><tr><td>X</td></tr></table>	X	<table><tr><td>X</td></tr></table>	X	<table><tr><td></td></tr></table>	
X						
X						
Unique or High Quality Habitat	<table><tr><td></td></tr></table>		<table><tr><td></td></tr></table>		<table><tr><td></td></tr></table>	

Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks: Based on a desktop review, a site visit on July 31, 2019 by Lochmueller Group, and the 2017 aerial map of the project area (Appendix B, B3), there is maintained roadside, residential lawn, forested, and agricultural field habitat within the project area. The maintained lawn of the roadside is dominated by Kentucky bluegrass (*Poa pratensis*), tall fescue (*Schedonorus arundinaceus*), and red fescue (*Festuca rubra*). The forested habitat is dominated by black willow (*Salix nigra*) and boxelder (*Acer negundo*). The agricultural field habitat is mainly dominated by planted crops.

It is anticipated that 0.23 acre of tree clearing will occur inside the construction limits and within 100 feet of the existing roadway. The tree clearing acreage listed in the USFWS Information for Planning and Consultation (IPaC) coordination, 0.34 acre, included trees within the ROW limits of the project. As planning for the project progressed, it was determined that all tree clearing was to occur within the construction limits and not extend to the ROW limits. Therefore, tree clearing was reduced for this project. The trees to be cleared mainly consist of the same trees listed above (black willow and boxelder). A total of 1.14 acres of ground disturbance is anticipated. Since the project will involve more than 1.0 acre of ground disturbance, an IDEM Rule 5 Permit will likely be required.

The avoidance of terrestrial habitat is not feasible because the proposed footprint is required for the construction activities of this project. These activities include construction access, removal of the existing bridge, construction of the new bridge, installation of new drainage culverts, guardrail replacement, installation of new riprap, and pavement activities.

The USACE did not respond to the early coordination letter. The IDNR DFW responded on March 9, 2020 with recommendations to avoid or minimize impacts to terrestrial habitat (Appendix C, C45 to C48). Their recommendations include revegetating all bare and disturbed areas with native vegetation, minimizing within project limits the clearing of trees and brush, time of year restrictions on tree clearing, and maintaining wildlife passage through the structure. All applicable agency recommendations are included in the *Environmental Commitments* section of this CE document.

An automated letter was generated from the IDEM website on February 5, 2020 (Appendix C, C9 to C16). The letter recommended that coordination occur with appropriate agencies to determine what permits may be required.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst	Yes	No
Is the proposed project located within or adjacent to the potential Karst Area of Indiana?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Are karst features located within or adjacent to the footprint of the proposed project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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If yes, will the project impact any of these karst features? ☐ ☐

Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks: Based on a desktop review, the project is located outside the designated karst region of Indiana, as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the USGS topographic map of the project area (Appendix B, B2) and the water resources map of the RFI report (Appendix E, E9), there are no karst features identified within or adjacent to the project area. In the early coordination response, the Indiana Geological Survey (IGS) did not indicate that karst features exist in the project area (Appendix C, C6 to C8). The IGS did state there is high potential to encounter bedrock resources and sand and gravel resources, and a moderate potential for liquefaction. Also, abandoned industrial mineral sand gravel pits, and a floodway were identified within 0.5 mile of the project area. The response from IGS was communicated with the designer on April 1, 2020. No impacts are expected.

Threatened or Endangered Species	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Within the known range of any federal species	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Any critical habitat identified within project area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Federal species found in project area (based upon informal consultation)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State species found in project area (based upon consultation with IDNR)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

	Yes	No
Is Section 7 formal consultation required for this action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: Based on a desktop review and the RFI report (Appendix E, E1 to E13) completed by Lochmueller Group on February 8, 2020, the IDNR Delaware County Endangered, Threatened, and Rare Species List has been checked and is included in Appendix E, E12 to E13. The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR DFW early coordination response dated March 9, 2020 (Appendix C, C45 to C48), the Natural Heritage Program's Database has been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Project information was submitted through the USFWS's IPaC portal, and an official species list was generated (Appendix C, C21 to C26). The project is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were found within or adjacent to the project area, other than the Indiana bat and NLEB.

The project qualifies for the *Range-wide Programmatic Informal Consultation* for the Indiana bat and NLEB, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on March 3, 2020, and based on the responses provided, the project was found to "May Affect – Not Likely to Adversely Affect" the Indiana bat and/or the NLEB. INDOT reviewed and verified the effect finding on March 3, 2020 and requested USFWS's review of the finding (Appendix C, C27 to C41). No response was received from USFWS within the 14-day review period; therefore, it was concluded that they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the *Environmental Commitments* section of this CE document.

In the IDNR DFW early coordination response (Appendix C, C45 to C48), they stated that bridge improvements could affect nesting birds and/or roosting bats. They stated that a survey for nesting birds should occur before construction and should occur during the nesting season, between May 7 and September 7. If nests with eggs, chicks, or parents are found, they should be undisturbed and bridge activities should be on

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hold until the nesting cycle is complete. They also suggested that bridge activities only occur between November 1 and March to avoid the summer roosting period for most bat species. Regardless of when bridge work is to occur, a bridge inspection should occur.

USFWS Bridge/Structure Assessment shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after July 31, 2021, an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. This is included as a firm commitment in the *Environmental Commitments* section of this document.

Migratory bird nests were not observed on the bridge. Migratory birds are protected under the federal Migratory Bird Treaty Act. An avoidance and minimization plan shall be developed by the contractor and approved by the Area Engineer and implemented prior to the start of and during the nesting season. At a minimum, the plan shall include provisions stating nests shall be removed prior to construction during the non-nesting season (September 8 to April 30). Nests cannot be disturbed during the nesting season (May 1 to September 7) without prior coordination with INDOT EWPO. If there are nests with eggs or young on the structure during the nesting season, the contractor shall make every effort to avoid impacts to the nests and notify the Project Engineer or Project Supervisor who will contact the assigned INDOT EWPO Specialist for Assistance

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at this site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

SECTION B – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area
Public Water System(s)
Residential Well(s)
Source Water Protection Area(s)
Sole Source Aquifer (SSA)

Presence

Impacts

Yes	No

If a SSA is present, answer the following:

Is the Project in the St. Joseph Aquifer System?
Is the FHWA/EPA SSA MOU Applicable?
Initial Groundwater Assessment Required?
Detailed Groundwater Assessment Required?

Yes

No

Remarks:

The project is located in Delaware County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the *FHWA/USEPA Sole Source Aquifer MOU* is not applicable to this project. Therefore, a detailed groundwater assessment is not needed, and no impacts are expected.

The IDEM Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on March 27, 2020 by Lochmueller Group. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

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The IDNR Water Well Web Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on March 27, 2020 by Lochmueller Group. No wells are located near this project area. Therefore, no impacts are anticipated.

Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by Lochmueller Group on March 27, 2020 and the RFI report; this project is located within an UAB. An early coordination letter was sent on February 5, 2020 to the Yorktown MS4 Coordinator. The MS4 Coordinator did not respond within the 30-day time frame. Further coordination occurred on June 18, 2020 to ensure that no issues relating to this project construction were identified. They responded on June 18, 2020 with no comments or questions regarding this project.

Based on a desktop review, a site visit on July 31, 2019 by Lochmueller Group, the 2017 aerial map of the project area (Appendix B, B3), and the design plans (Appendix B, B17), no public water systems were identified. Therefore, no impacts are expected.

Flood Plains

Longitudinal Encroachment
Transverse Encroachment
Project located within a regulated floodplain
Homes located in floodplain within 1000' up/downstream from project

Presence

X
X
X

Impacts

Yes	No
X	
X	
	X

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks:

Based on a desktop review of the IDNR Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) by Lochmueller Group on March 27, 2020, and the waters resources map of the RFI report (Appendix E, E9); this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, F10). An early coordination letter was sent on February 5, 2020 to the local Floodplain Administrator. The floodplain administrator did not respond within the 30-day time frame. This project qualifies as a Category 4 per the current *INDOT CE Manual*, which states:

Five homes are located within the base floodplain elevation within 1,000 feet upstream and no homes are located within the base floodplain within 1,000 feet downstream. The proposed structure will have an effective capacity such that backwater surface elevations are not expected to substantially increase. As a result, there will be no substantial adverse impacts on natural and beneficial floodplain values; there will be no substantial change in flood risks; and there will be no substantial increase in potential for interruption or termination of emergency service or emergency evacuation routes; therefore, it has been determined that this encroachment is not substantial. A hydraulic design study that addresses various structure size alternates will be completed during the preliminary design phase. A summary of this study will be included with the Field Check Plans.

Farmland

Agricultural Lands
Prime Farmland (per NRCS)

Presence

X
X

Impacts

Yes	No
X	
X	

Total Points (from Section VII of CPA-106/AD-1006* 143

**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

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Remarks: Based on a desktop review, a site visit on July 31, 2019 by Lochmueller Group, and the 2017 aerial map of the project area (Appendix B, B3), the project will convert 0.15 acre of farmland as defined by the Farmland Protection Policy Act. An early coordination letter was sent on February 5, 2020 to Natural Resources Conservation Service (NRCS). Coordination with NRCS resulted in a score of 143 on the NRCS-CPA-106 (Appendix C, C18). NRCS's threshold score for significant impacts to farmland that result in the consideration of alternatives is 160. Since this project score is less than the threshold, no significant loss of prime, unique, statewide, or local important farmland will result from the project. No alternatives, other than those previously discussed in this document, will be investigated without re-evaluating impacts to prime farmland. On the NRCS-CPA-106 form, NRCS stated that 0.41 acre of prime farmland is within the project area. This acreage represents all of the land on the east side of Nebo Road, with the exception of 0.15 acre of residential lawn. The forested areas along York Prairie Creek are also included in their acreage of prime farmland.

SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates	N/A
Minor Projects PA Clearance	B	1 & 12	February 25, 2020	N/A

Eligible and/or Listed Resource Present

Results of Research

Archaeology	
NRHP Buildings/Site(s)	
NRHP District(s)	
NRHP Bridge(s)	

Project Effect

No Historic Properties Affected ☐ No Adverse Effect ☐ Adverse Effect ☐

Documentation Prepared

Documentation (mark all that apply)

	ES/FHWA Approval Date(s)	SHPO Approval Date(s)
Historic Properties Short Report		
Historic Property Report		
Archaeological Records Check/ Review		
Archaeological Phase Ia Survey Report	X	February 25, 2020
Archaeological Phase Ic Survey Report		N/A
Archaeological Phase II Investigation Report		
Archaeological Phase III Data Recovery		
APE, Eligibility and Effect Determination		
800.11 Documentation		

Memorandum of Agreement (MOA) ☐

MOA Signature Dates (List all signatories)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

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Remarks: On February 25, 2020, the INDOT Cultural Resources Office (CRO) determined that this project falls within the guidelines of Category B, Types 1 and 12 under the Minor Projects Programmatic Agreement (MPPA) (Appendix D, D1 to D6). The projects that fall under the aforementioned MPPA categories are as follows:

B-1: Replacement, repair, or installation of sidewalks associated with roadway work in undisturbed soils where an archaeological investigation was performed and no National Register for Historic Places (NRHP) listed or eligible resources are found within or near the project area.

B-12: Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects where an archaeological investigation was performed and no NRHP listed or eligible resources are found within or near the project area and the latest Historic Bridge Inventory identified the bridge as non-historic.

A Phase Ia Archaeological Short Report was completed for this project by Cultural Resources Analytics, Inc. on November 22, 2019. No archaeological sites were encountered within the survey area. No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

Publicly owned park
Publicly owned recreation area
Other (school, state/national forest, bikeway, etc.)

Presence

Use

Yes	No

Evaluations Prepared

Programmatic Section 4(f)*
“De minimis” Impact*
Individual Section 4(f)

FHWA Approval date

--

Wildlife & Waterfowl Refuges

National Wildlife Refuge
National Natural Landmark
State Wildlife Area
State Nature Preserve

Presence

Use

Yes	No

Evaluations Prepared

Programmatic Section 4(f)*
“De minimis” Impact*
Individual Section 4(f)

FHWA Approval date

--

Historic Properties

Sites eligible and/or listed on the NRHP

Presence

--

Use

Yes	No

Evaluations Prepared

--

FHWA

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County DelawareRoute Nebo RoadDes. No. 1700680Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

Approval date

--

*FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, "de minimis" and Individual Section 4(f) evaluations please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreational areas, wildlife/waterfowl refuges, and NRHP eligible or listed historical properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, a site visit on July 31, 2019 by Lochmueller Group, the 2017 aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E8) there are two Section 4(f) resource within the 0.5 miles search radius. Neither of these are located within or adjacent to the project area. Additionally, a privately-owned golf course, the Players Club Golf Course, is located adjacent to the project area. As a privately-owned recreational facility, this golf course is not subject to protection under Section 4(f). Therefore, there are no Section 4(f) resources within or adjacent to the project area. Therefore, no impacts are expected.

The project will add a 6-foot wide sidewalk on the east side of the new bridge. However, this sidewalk is meant to connect with future multimodal facilities that may be constructed independent of this bridge project. It should be noted that the sidewalk will only be present along the east side of the bridge and will not extend past the limits of the new structure.

Section 6(f) Involvement

PresenceUse

Section 6(f) Property

Yes

No

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--

--

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF) which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of Section 6(f) property on the LWCF website at (<https://www.lwcfcoalition.com/tools>) revealed a total of three properties in Delaware County (Appendix J, J1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to Section 6(f) resources as a result of this project.

SECTION E – Air Quality

Air Quality

Conformity Status of the Project

Is the project in an air quality non-attainment or maintenance area?

Yes

No

X

--

If YES, then:

Is the project in the most current MPO TIP?

X

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Is the project exempt from conformity? ☒ ☐

If the project is NOT exempt from conformity, then:

Is the project in the Transportation Plan (TP)? ☐ ☐

Is a hot spot analysis required (CO/PM)? ☐ ☐

Level of MSAT Analysis required?

Level 1a ☒ Level 1b ☐ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Remarks:

This project is included in the Fiscal Year (FY) 2020-2024 Delaware-Muncie Metropolitan Plan Commission's Transportation Improvement Program (TIP) and the FY 2020-2024 Statewide Transportation Improvement Program (STIP) (Appendix H, H1 to H2).

This project is located in Delaware County, which is currently a maintenance area for 8-hour ozone according to the IDEM Office of Air Quality website (<https://www.in.gov/idem/airquality/2339.htm>). This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION F – NOISE

Noise

Yes

No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

☐
☒

No

Yes/ Date

ES Review of Noise Analysis

☐
☐

Remarks:

This is a Type III project. In accordance with 23 CFR 772 and the current INDOT *Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

Will the proposed action result in substantial impacts to community cohesion?

Will the proposed action result in substantial impacts to local tax base or property values?

Will construction activities impact community events (festivals, fairs, etc.)?

Does the community have an approved transition plan?

If No, are steps being made to advance the community's transition plan?

Does the project comply with the transition plan? (explain in the remarks box)

Yes

No

☒
☐
☐
☒
☐
☒
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☒
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☐
☐
☒
☐

Remarks:

The project will ultimately be beneficial to local business and properties due to improvements of deteriorating roadway conditions and will not substantially change access to properties within the area, etc. Overall, the negative impacts to property owners and local businesses within the project area will be minimal and will

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consist primarily of short-term construction impacts. No relocations are expected. Property owners will be provided access throughout the duration of the project to reduce impacts as much as possible. The project is not anticipated to result in substantial impacts to community cohesion, because it will not change access to properties within the area. The proposed project is not expected to impact the surrounding community or cause economic impacts to the surrounding area. Therefore, this project will have minimal or no negative impacts to the community or local economy.

According to the Fairs and Festivals website (www.indianafestivals.org) accessed on April 6, 2020 by Lochmueller Group, there are five fairs and festivals scheduled within 10 miles of the project. These festivals take place in the City of Muncie and will not likely be impacted by the project.

The MOT may pose delays and temporary inconveniences to traveling motorists (including school buses and emergency services); however, all inconveniences will cease upon project completion. The MOT for the project is not anticipated to impact access to community events. The project sponsor will be responsible for contacting school districts and emergency services at least two weeks prior to any construction activities that would block or limit access, this is included as a commitment in the *Environmental Commitments* section of this CE document.

The *Americans with Disabilities Act (ADA) Transition Plan* for Delaware County was approved and implemented on February 21, 2014. The project will comply with the published ADA Transition Plan and will not create any additional barriers to access. No existing pedestrian facilities exist within the project area. The project will add a 6-foot wide sidewalk on the east side of the new bridge that will not extend past the limits of the new structure. However, this sidewalk is meant to connect with future multimodal facilities that may be constructed independent of this bridge project. Therefore, no pedestrian facilities will be impacted by the project.

An early coordination letter was sent to the Delaware-Muncie Metropolitan Planning Commission on February 5, 2020 (Appendix C, C1 to C5). No response was received.

Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such other actions.

This project will not add substantial capacity to the existing roadway network or provide additional access to any currently undeveloped area. Therefore, the project is not expected to increase development in the area or result in substantial indirect or cumulative impacts.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Based on a desktop review, a site visit on July 31, 2019 by Lochmueller Group, the 2017 aerial map of the project area (Appendix B, B3), the preliminary design plans (Appendix B, B17) and the RFI report (Appendix E, E8) there is an overhead electrical utility within the project area. Utility coordination has begun and will continue through project development. Access to all properties will be maintained during construction. Therefore, no impacts are expected.

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Early coordination information was sent to Yorktown Community Schools, Delaware County Board of Commissioners, Delaware County Council, Delaware County Highway Department, Delaware County Emergency Medical Service, Delaware County Sheriff's Department, Yorktown Fire Department, and Yorktown Police Department on February 5, 2020 (Appendix C, C1 to C5). None of the agencies responded to the early coordination letter.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

Yes No

During the development of the project were EJ issues identified?

X	
X	

Does the project require an EJ analysis?

X	
X	

If YES, then:

Are any EJ populations located within the project area?

	X
	X

Will the project result in adversely high or disproportionate impacts to EJ populations?

	X
	X

Remarks:

Under FHWA Order 6640.23A, FHWA and the project sponsor, as recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT *Categorical Exclusion Manual*, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent ROW. This project will require 0.89 acre of permanent ROW. Therefore, an EJ analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exist and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city, or town and is called the community of comparison (COC). In this project, the COC is Delaware County, Indiana. The community that overlaps the project limits is called the affected community (AC). In this project, the AC is Census Tract 24.02. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2017 American Community Survey (ACS) was obtained from the U.S. Census Bureau website (<https://factfinder.census.gov/>) on March 5, 2020 by Lochmueller Group. The data collected for minority and low-income populations within the AC are summarized in the table below.

Table: Minority and Low-Income Data (2017 ACS Data)		
	COC	AC
	Delaware County, Indiana	Census Tract 24.02, Delaware County, Indiana
MINORITY		
Percent Minority	12.9%	6.4%
125% of COC	16.2%	AC < 125% COC
EJ Population of Concern?		No
LOW-INCOME		
Percent low-income	21.2%	5.7%
125% of COC	26.4%	AC < 125% COC
EJ Population of Concern?		No

The AC, Census Tract 24.02, has a percent minority of 6.4% which is below 50% and is below the 125% COC threshold. Therefore, the AC does not contain minority populations of EJ concern.

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The AC, Census Tract 24.02, has a percent low-income of 5.7% which is below 50% and is below the 125% COC threshold. Therefore, the AC does not contain low-income populations of EJ concern.

The census data sheets, map, and calculations can be found in Appendix I, I1 to I6. No further EJ analysis is warranted.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Is a Business Information Survey (BIS) required?

Is a Conceptual Stage Relocation Study (CSRS) required?

Has utility relocation coordination been initiated for this project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks: No relocations of people, businesses, or farms will take place as a result of this project.

Since utilities exist within the project area that may have to be relocated after construction, coordination has begun and will continue through project development in order to minimize impacts to utilities.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Documentation

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

No Yes/ Date

ES Review of Investigations

March 5, 2020

Include a summary of findings for each investigation.

Remarks: Based on a review of GIS and available public records, an RFI was approved on March 5, 2020 by INDOT Site Assessment and Management (Appendix E, E1 to E13). One leaking underground storage tank (LUST) and two solid waste landfills are located within 0.5 mile of the project, and no sites are located within the project area. However, no hazmat sites were identified in or within 0.5 mile of the project area that will impact the project. The LUST site is 0.39 mile from the project and the nearest landfill is 0.43 mile from the project. No impacts are expected. Further investigation for hazardous material concerns is not required at this time.

SECTION I – PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Individual Permit (IP)

Nationwide Permit (NWP)

Regional General Permit (RGP)

<input type="checkbox"/>
<input type="checkbox"/>
<input checked="" type="checkbox"/>

This is page 21 of 25 Project name: Delaware County Bridge No. 125, Bridge Replacement Project Date: July 14, 2020

Indiana Department of Transportation

County Delaware Route Nebo Road Des. No. 1700680

	Pre-Construction Notification (PCN)	<input type="checkbox"/>
	Other	<input type="checkbox"/>
	Wetland Mitigation required	<input type="checkbox"/>
	Stream Mitigation required	<input type="checkbox"/>
IDEM		
	Section 401 WQC	<input checked="" type="checkbox"/>
	Isolated Wetlands determination	<input type="checkbox"/>
	Rule 5	<input checked="" type="checkbox"/>
	Other	<input type="checkbox"/>
	Wetland Mitigation required	<input type="checkbox"/>
	Stream Mitigation required	<input type="checkbox"/>
IDNR		
	Construction in a Floodway	<input checked="" type="checkbox"/>
	Navigable Waterway Permit	<input type="checkbox"/>
	Lake Preservation Permit	<input type="checkbox"/>
	Other	<input type="checkbox"/>
	Mitigation Required	<input type="checkbox"/>
US Coast Guard Section 9 Bridge Permit		<input type="checkbox"/>
Others (Please discuss in the remarks box below)		<input type="checkbox"/>

Remarks:

A total of 172 feet (0.07 acre below OHWM) of York Prairie Creek will be impacted by the project. Impacts will be limited to the portion of the creek within the construction limits of the project. A USACE Section 404 RGP and IDEM Section 401 WQC will be required due to the impacts to York Prairie Creek. A formal jurisdictional determination has not yet been made by the USACE, which will be required during the permitting phase.

Due to the cumulative impacts of 172 linear feet and 0.07 acre, mitigation is not likely required for the USACE Section 404 RGP or the IDEM Section 401 WQC, but will be determined during the permitting process.

Due to the project location within a regulated floodway, the acquisition of a Construction in a Floodway permit from the IDNR will be required.

The project may disturb up to 1.14 acres of land. Therefore, the project is expected to exceed the minimal guidelines of soil disturbance and an IDEM Rule 5 Notice of Intent will be required.

This project lies within the Yorktown UAB. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. Coordination with the Yorktown MS4 coordinator has occurred and they responded with no questions or comments regarding this project.

Applicable recommendations provide by IDNR are included in the *Environmental Commitments* section of this CE document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s), and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks:

Firm:

- If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)

Indiana Department of Transportation

County Delaware Route Nebo Road Des. No. 1700680

2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. Any work in a wetland area within INDOT's right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineer or IDEM permit. (INDOT ESD)
4. **General AMM 1:** Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
5. **Tree Removal AMM 1:** Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
6. **Tree Removal AMM 2:** Apply time of year restrictions (November 1 through March 1) for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/ rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS)
7. **Tree Removal AMM 3:** Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
8. **Tree Removal AMM 4:** Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year. (USFWS)
9. If box or pipe culverts are used, the bottoms should be buried to a minimum of 6" (or 20% of the culvert height/pipe diameter, whichever is greater up to a maximum of 2') below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should: span the entire channel width (a minimum of 1.2 times the bankful width); maintain the natural stream substrate within the structure; have a minimum openness ratio (height x width/length) of 0.25; and have stream depth and water velocities during low-flow conditions that are approximate to those in the natural stream channel. The new, replacement, or rehabbed structure should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. (IDNR DFW)
10. Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Riprap may be used only at the toe of the sideslopes up to the ordinary high water mark (OHWM). The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Eastern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. (IDNR DFW)
11. Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees). (IDNR DFW)
12. Migratory bird nests were not observed on the bridge. Migratory birds are protected under the federal Migratory Bird Treaty Act. An avoidance and minimization plan shall be developed by the contractor and approved by the Area Engineer and implemented prior to the start of and during the nesting season. At a minimum, the plan shall include provisions stating nests shall be removed prior to construction during the non-nesting season (September 8 to April 30). Nests cannot be disturbed during the nesting season (May 1 to September 7) without prior coordination with INDOT EWPO. If there are nests with eggs or young on the structure during the nesting season, the contractor shall make every effort to avoid impacts to the nests and notify the Project Engineer or Project Supervisor who will contact the assigned INDOT EWPO Specialist for Assistance (INDOT EWPO)

Indiana Department of Transportation

County Delaware Route Nebo Road Des. No. 1700680

13. A USFWS Bridge/Structure Assessment shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after July 31, 2021, an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. (INDOT ESD)

For Further Consideration:

14. Avoid all work within the inundated part of the stream channel during the fish spawning season (April 1 through June 30); except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams. (USFWS)
15. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels, and diversion fencing. (USFWS)
16. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS)
17. Restrict below low-water work in streams to placement of culverts, piers, pilings, and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. (USFWS)
18. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting from April 1 through September 30. (IDNR DFW)
19. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure. (IDNR DFW)
20. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds. (IDNR DFW)
21. Operate equipment used to replace the bridge from the existing roadway. (IDNR DFW)
22. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR DFW)

Indiana Department of Transportation

County Delaware Route Nebo Road Des. No. 1700680

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks:

Early coordination with the regulatory agencies was completed on February 5, 2020 (Appendix C, C1 to C5). If no response was received, it was assumed the agency did not feel the project will result in substantial impacts. The following agencies/individuals were contacted during the coordination phase.

	Agency	Date of Response(s)
1.	USACE, Louisville District	No Response
2.	USFWS, Bloomington Field Office	March 3, 2020
3.	USDA, NRCS	February 20, 2020
4.	National Park Service, Midwest Regional Office	No Response
5.	U.S. Department of Housing and Urban Development	No Response
6.	FHWA, Indiana Division	No Response
7.	IDNR, Division of Fish and Wildlife	March 9, 2020
8.	IDEM (electronic submission)	February 5, 2020
9.	Indiana Geological Survey	February 5, 2020
10.	INDOT, Office of Public Involvement	No Response
11.	INDOT, Environmental Services	February 27, 2020
12.	INDOT, Greenfield District Environmental Scoping Manager	No Response
13.	Delaware County Board of Commissioners	No Response
14.	Delaware County Council	No Response
15.	Delaware County Highway Department	No Response
16.	Delaware County Drainage Board	No Response
17.	Delaware County Surveyor's Office	No Response
18.	Delaware County Emergency Management Agency	No Response
19.	Delaware County Emergency Medical Service	No Response
20.	Delaware County Sheriff's Department	No Response
21.	Yorktown/Mt. Pleasant Township Committee	No Response
22.	Delaware-Muncie Metropolitan Plan Commission	No Response
23.	Yorktown Community School Corporation	No Response
24.	Yorktown Street Department	No Response
25.	Yorktown Town Council	No Response
26.	Yorktown Police Department	No Response
27.	Yorktown Fire Department	No Response
28.	Yorktown Parks Department	No Response
29.	The Player's Club Golf Course	No Response
30.	Bureau of Water Quality – Muncie Sanitary District	June 18, 2020

Appendix A: INDOT Supporting Documentation

Threshold Chart.....	A1
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Appendix B: Graphics

General Location Map.....	B1
USGS Muncie West, Indiana Quadrangle Topographic Map	B2
Project Aerial (2017)	B3
Photo Location Map (2017)	B4
Site Photographs.....	B5-B11
Preliminary Plan Sheets.....	B12-B20

Appendix C: Early Coordination

Sample Early Coordination Letter (February 5, 2020).....	C1-C5
Indiana Geological Survey	
Electronic Response (February 5, 2020).....	C6-C8
Indiana Department of Environmental Management	
Electronic Response (February 5, 2020).....	C9-C16
Natural Resources Conservation Service	
Response Letter (February 23, 2020).....	C17
Completed NRCS-CPA-106 Form.....	C18
Indiana Department of Transportation (INDOT), Environmental Services	
Response Email (February 27, 2020).....	C19-C20
United States Fish and Wildlife Service	
IPaC Official Species List (March 3, 2020).....	C21-C26
IPaC Concurrence Verification Letter (March 3, 2020).....	C27-C41
Bridge/Structure Assessment Form (July 31, 2019)	C42-C43
INDOT IPaC Approval (March 3, 2020)	C44
Indiana Department of Natural Resources (IDNR), Division of Fish and Wildlife	
Response Letter (March 9, 2020).....	C45-C48

Appendix D: Section 106 of the National Historic Preservation Act (NHPA)

MPPA Project Assessment Form	D1-D5
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Appendix E: Red Flag Investigation

Red Flag Investigation.....	E1-E13
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Appendix F: Water Resources

Waters of the U.S. Determination Report.....	F1-F7
Water Resources Map	F8
NWI Wetlands Map	F9
FEMA Floodplain Map	F10
USGS StreamStats Map	F11
Soil Survey	F12-F17
Preliminary Jurisdictional Determination	F18-F21

Appendix G: Public Involvement

Notice of Survey.....	G1
INDOT Notice of Survey Attachment.....	G2

Appendix H: Air Quality

Excerpt from the Delaware-Muncie Metropolitan Planning Commission 2020-2023 TIP	H1
Relevant pages from the INDOT 2020-2024 STIP	H2

Appendix I: Environmental Justice (EJ) Analysis

EJ Analysis Map.....	I1
Data Calculation Table.....	I2

Population Data	I3-I6
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Appendix J: Other Information

Land and Water Conservation Fund Grants: Delaware County, Indiana	J1
INDOT Bridge Inspection Report	J2-J10

Categorical Exclusion

Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	"No Effect", "Not likely to Adversely Affect" (Without AMMs ⁴ or with AMMs required for all projects ⁵)	"Not likely to Adversely Affect" (With any other AMMs)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	"No Effect", "Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level <ul style="list-style-type: none"> • District Env. Supervisor • Env. Services Division • FHWA 	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes Yes	Yes Yes Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User's Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as "required for all projects".

⁶Potential for causing a disproportionately high and adverse impact.

⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

Categorical Exclusion

Appendix B

Graphics



Legend

Project Area

Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, (c) OpenStreetMap contributors, and the GIS User Community

General Location Map

Des. No. 1700680

0 0.75 1.5
Miles

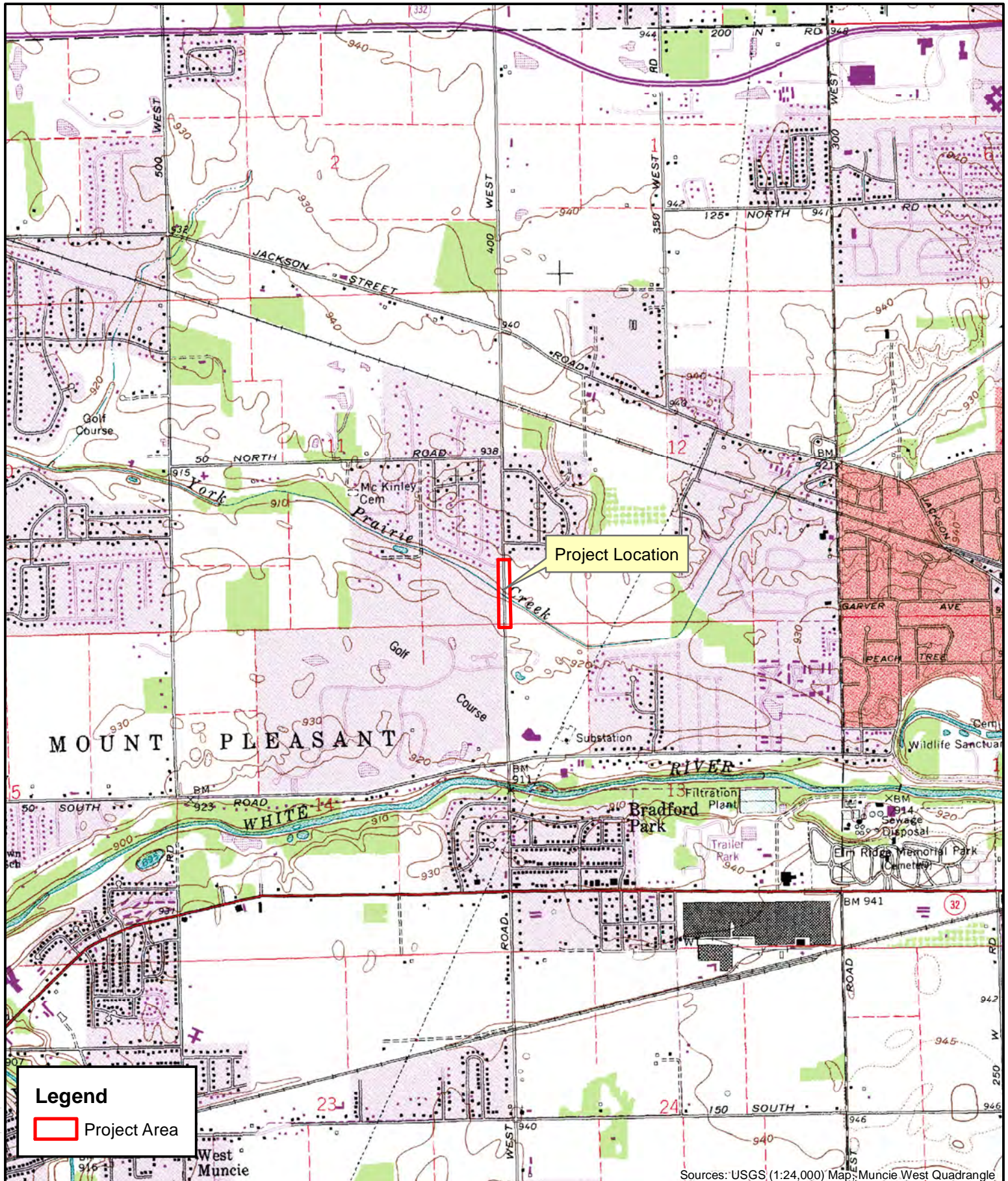


County: Delaware
Township: Mt. Pleasant
State: Indiana

Delaware County Bridge No. 125
Nebo Road over York Prairie Creek
Created: 5/13/2020, C Kunkel



3502 Woodview Trace, Suite 150
Indianapolis, IN 46268
Phone: (317) 222-3880
Fax: (317) 222-3881



LOCHMUELLER GROUP

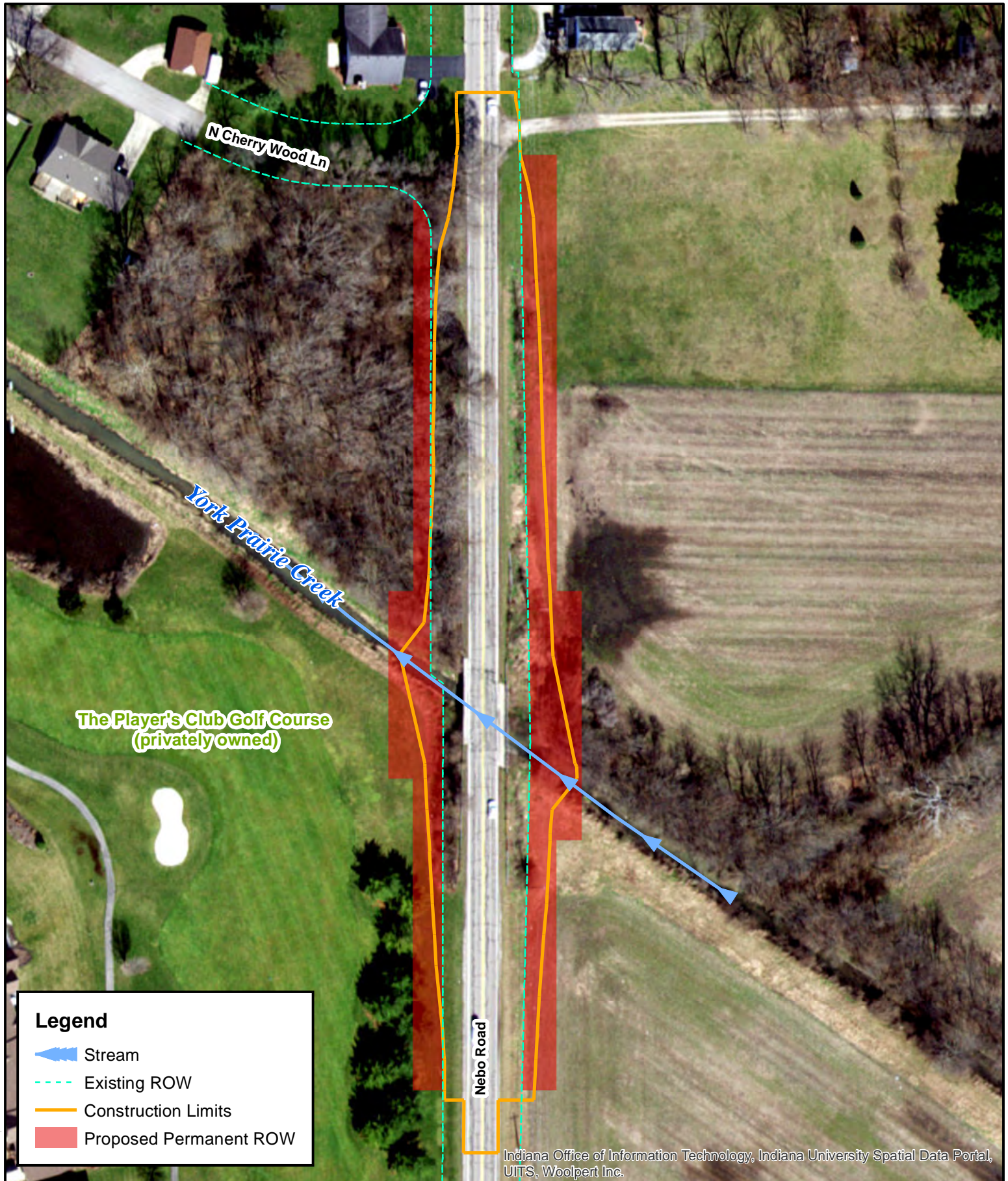
3502 Woodview Trace, Suite 150
Indianapolis, IN 46268
Phone: (317) 222-3880
Fax: (317) 222-3881

USGS Topographic Map
Muncie West Quadrangle
Des. No. 1700680





0 0.25 0.5 Miles

County: Delaware
Township: Mt. Pleasant
State: Indiana

Delaware County Bridge No. 125
Nebo Road over York Prairie Creek
Created: 5/13/2020, C Kunkel



Legend

-  Stream
-  Existing ROW
-  Construction Limits
-  Proposed Permanent ROW

Aerial Map (2017)

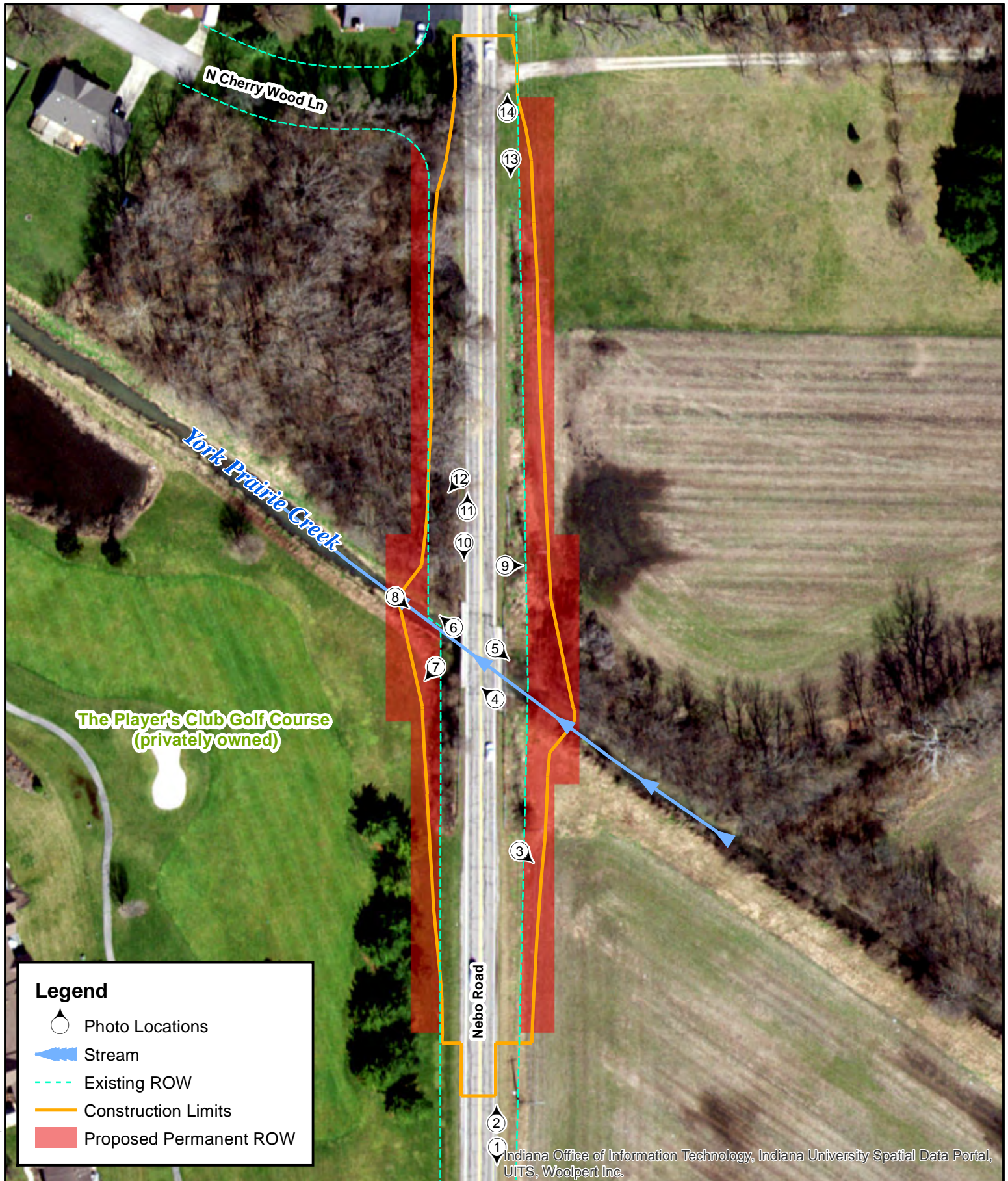
Des. No. 1700680

0 50 100
Feet



County: Delaware
Township: Mt. Pleasant
State: Indiana

Delaware County Bridge No. 125
Nebo Road over York Prairie Creek
Created: 5/13/2020, C Kunkel



S:_2016\216-0303\Enviro\Map\Map\Photo map.mxd

Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.

LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
Indianapolis, IN 46268
Phone: (317) 222-3880
Fax: (317) 222-3881



1. Looking south along Nebo Road



2. Looking north along Nebo Road



3. Looking southeast into agricultural field



4. Looking northwest (downstream) along York Prairie Creek at bridge structure



5. Looking southeast (upstream) along York Prairie Creek from bridge



6. Looking northwest (downstream) along York Prairie Creek from bridge



7. Looking southwest toward The Player's Club Golf Course



8. Looking southeast (upstream) along York Prairie Creek toward bridge



9. Looking east toward agricultural field



10. Looking southwest at wooded area



11. Looking north along Nebo Road



12. Looking south along Nebo Road



13. Looking north along Nebo Road



14. Looking south along Nebo Road

PROJECT	DESIGNATION
1700680	1700680
CONTRACT	BRIDGE FILE
B-40331	DELAWARE 00125 B

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
DELAWARE 00125 B	Composite Prestressed Box Beam	1 Span: 60'-0" Skew: 35° Rt.	York Prairie Creek	17+55.20 Line "PR-A"

ADDITIONAL RIGHT OF WAY
REQUIRED FOR THIS PROJECT

PRELIMINARY FIELD CHECK
APRIL 1, 2020

APPROVED BY:

DELAWARE COUNTY BOARD OF COMMISSIONERS

James King, President

Date

Sherry Riggin, Vice-President

Date

Shannon Henry, Member

Date

ATTEST BY:

DELAWARE COUNTY AUDITOR

Steven G. Craycraft, County Auditor

Date

RECOMMENDED FOR APPROVAL BY:

DELAWARE COUNTY HIGHWAY DEPARTMENT

Angela Moyer, Project Director/ Administrator, Employee in Responsible Charge

Date

INDIANA DEPARTMENT OF TRANSPORTATION



BRIDGE PLANS

FOR SPANS OVER 20 FEET

ROUTE: NEBO ROAD

AT: YORK PRAIRIE CREEK

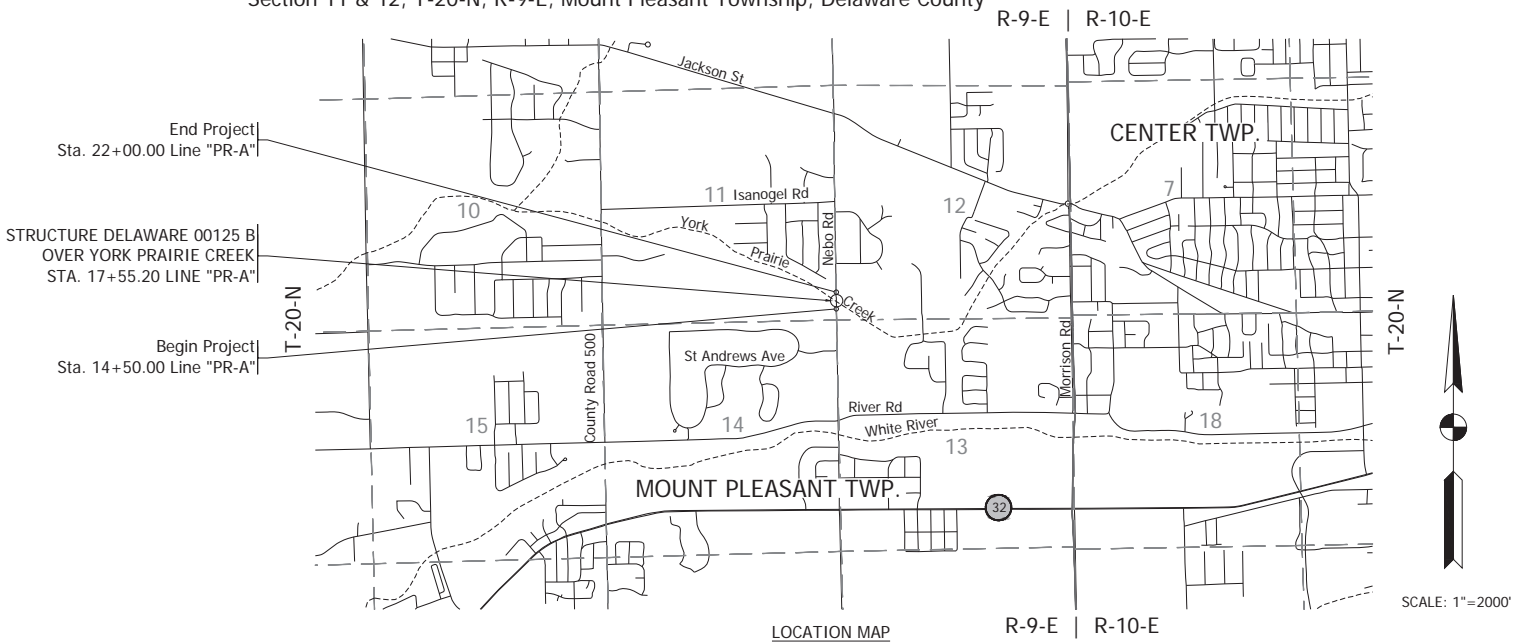
PROJECT NO.

1700680 P.E.

1700680 R/W

1700680 CONST.

Bridge Replacement, Other Construction on Nebo Road Over York Prairie Creek
Located 0.50 Miles North of County Road 50 South
Section 11 & 12, T-20-N, R-9-E, Mount Pleasant Township, Delaware County

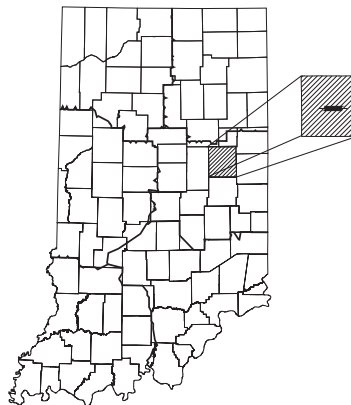


TRAFFIC DATA

A.A.D.T.	(2021)	10,003 V.P.D.
A.A.D.T.	(2041)	14,865 V.P.D.
D.H.V	(2041)	856 V.P.H.
DIRECTIONAL DISTRIBUTION		50 %
TRUCKS		5% A.A.D.T. 5% D.H.V.

DESIGN DATA

DESIGN SPEED	40 M.P.H.
PROJECT DESIGN CRITERIA	RECONSTRUCTION (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MINOR ARTERIAL
RURAL/URBAN	URBAN (INTERMEDIATE)
TERRAIN	LEVEL
ACCESS CONTROL	NONE



PROJECT LOCATION SHOWN BY
Delaware County

LATITUDE: 40° 11' 39" LONGITUDE: 85° 27' 42"

BRIDGE LENGTH: 0.012 MI.
ROADWAY LENGTH: 0.130 MI.
TOTAL LENGTH: 0.142 MI.
MAX. GRADE: 2.00 %

HUC 12: 051202010305
HUC 14: 05120201030010



PLANS PREPARED BY:	Lochmueller Group, Inc.	574-334-5460 PHONE NUMBER
CERTIFIED BY:	REGISTERED PROFESSIONAL ENGINEER STATE OF INDIANA NO. 10302082	--/--/-- DATE
CERTIFIED BY:	REGISTERED PROFESSIONAL ENGINEER STATE OF INDIANA NO. 19800103	--/--/-- DATE
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS

	BRIDGE FILE		
	DELAWARE 00125 B		
	DESIGNATION		
	1700680		
SURVEY BOOK	SHEET		
ELECTRONIC	1	of	17
CONTRACT	PROJECT		
B-40331	1700680		

UTILITIES	
American Electric Power Joshua Adams 7775 Walton Parkway New Albany, OH 43054 314-933-2297	Vectren 16000 Allisonville Rd. Noblesville, IN 46061 317-718-3639
AT&T - Distribution Matt Spindler 240 N Meridian St. Indianapolis, IN 46204 317-265-3050	Town of Yorktown Pete Olson 9800 W Smith St. Yorktown, IN 47396 765-759-4016
Comcast Cable Will Morris 317-275-6443	
Muncie Sanitary District John Anderson 5120 Kilgore Ave. Muncie, IN 47304 765-747-4879 x232	



GENERAL NOTES

[illegible]

Survey Reference Ties

<p>300 NORTH: 170186.4280 EAST: 770106.0880</p> <p>5/8" REBAR W/CAP NPES TRAVERSE PT.</p> <p>1. MAG NAIL SET IN 8" PINE TREE, 1' ABOVE GRADE - 140.57' 2. MAG NAIL SET IN 8" PINE TREE, 1' ABOVE GRADE - 63.16' 1.5" IRON ROD AT SECTION CORNER FLUSH - 61.20'</p> <p>NPES CONTROL POINT</p>	<p>301 NORTH: 170614.2740 EAST: 770163.9620</p> <p>5/8" REBAR W/CAP NPES TRAVERSE PT.</p> <p>1. MAG NAIL SET POWER POLE, 5' ABOVE GRADE - 158.13' 2. MAG NAIL SET POWER POLE, 5' ABOVE GRADE - 71.46' 3. BENCH TIE SET IN POWER POLE, 1' ABOVE GRADE - 122.72'</p> <p>NPES CONTROL POINT</p>	<p>302 NORTH: 170882.6510 EAST: 770167.6400</p> <p>5/8" REBAR W/CAP NPES TRAVERSE PT.</p> <p>1. MAG NAIL SET POWER POLE, 5' ABOVE GRADE - 104.08' 2. MAG NAIL SET POWER POLE, 5' ABOVE GRADE - 113.04' 3. MAG NAIL SET IN 10" TREE, 4' ABOVE GRADE - 36.60'</p> <p>NPES CONTROL POINT</p>	<p>4 NORTH: 172901.4050 EAST: 770150.1100</p> <p>FOUND 1" AXLE IN CIRCLE MONUMENT BOX, 2' BELOW GRADE</p> <p>1. MAG NAIL IN PWP, 1' ABOVE GRADE - 29.80' 2. MAG NAIL IN PWP, 1' ABOVE GRADE - 34.40' 3. MAG NAIL IN PWP, 1' ABOVE GRADE - 34.40'</p> <p>E 1/4 CORNER, SEC 11, T20N, R9E</p>
<p>6 NORTH: 172681.0940 EAST: 767428.8560</p> <p>FOUND PK NAIL, 1' BELOW GRADE</p> <p>1. ASHTON LAND SURVEYOR PROPERTY CORNER - 72.90' 2. PK NAIL IN NW SIDE PWP - 105.12' 3. PK NAIL IN NW SIDE PWP - 67.62' 4. CENTER OF 8" WOODEN TRAIL - 23.57' 5. PK NAIL IN ROAD - 70.60'</p> <p>CENTER, SEC 11, T20N, R9E</p>	<p>7 NORTH: 172827.5780 EAST: 764835.6700</p> <p>FOUND RAILROAD SPIKE, 3' BELOW GRADE</p> <p>1. MAG NAIL IN PWP, 1' ABOVE GRADE - 46.30' 2. MAG NAIL IN PWP, 1' ABOVE GRADE - 65.30' 3. MAG NAIL IN PWP, 1' ABOVE GRADE - 51.30'</p> <p>W 1/4 CORNER, SEC 11, T20N, R9E</p>	<p>8 NORTH: 170183.6790 EAST: 764824.0210</p> <p>FOUND MAG NAIL, 5' BELOW GRADE</p> <p>1. PK NAIL FOUND IN W SIDE PWP - 96.13' 2. PK NAIL FOUND IN TOP OF 12" DIA. CONCRETE POST - 15.80' 3. PK NAIL FOUND IN W SIDE PWP - 64.79' 4. PK NAIL FOUND IN W SIDE PWP - 103.42'</p> <p>SW CORNER, SEC 11, T20N, R9E</p>	<p>9 NORTH: 167530.0430 EAST: 764826.0610</p> <p>FOUND RAILROAD SPIKE, 6' BELOW GRADE</p> <p>1. TOP NUT OF FIRE HYDRANT - 62.90' 2. SW CORNER OF SOUTH WALL OF PAVED TRAIL - 48.90' 3. SE CORNER OF NORTH WALL OF PAVED TRAIL - 35.70'</p> <p>W1/4 CORNER, SEC 14, T20N, R9E</p>
<p>10 NORTH: 170231.7580 EAST: 770147.3210</p> <p>FOUND 1.5" IRON ROD IN CONCRETE, FLUSH</p> <p>1. 5/8" REBAR WINCAPS CONTROL CAP FLUSH - 41.20' 2. MAG NAIL FOUND IN PWP, 2' ABOVE GRADE - 121.90' 4. MAG NAIL FOUND IN PWP, 1' ABOVE GRADE - 102.00'</p> <p>NW CORNER, SEC 13, T20N, R9E</p>	<p>12 NORTH: 172874.1590 EAST: 772813.5190</p> <p>FOUND 5/8" REBAR W/ASHTON CAP</p> <p>1. MAG NAIL ON RAILROAD TIE N SIDE OF TRACK - 45.30' 2. MAG NAIL ON RAILROAD TIE N SIDE OF TRACK - 79.10' 3. MAG NAIL ON RAILROAD TIE N SIDE OF TRACK - 43.40'</p> <p>CENTER, SEC 12, T20N, R9E</p>	<p>19 NORTH: 170147.9120 EAST: 775476.0370</p> <p>FOUND MAG SPIKE, 5' BELOW GRADE</p> <p>1. PK NAIL FOUND IN PWP - 68.14' 2. PK NAIL FOUND IN PWP - 17.02' 3. PK NAIL FOUND IN PWP - 62.16' 4. MAG SPIKE FOUND IN ROAD - 67.46'</p> <p>SE CORNER, SEC 12, T20N, R9E</p>	<p>20 NORTH: 170215.4180 EAST: 775476.2980</p> <p>FOUND MAG SPIKE, 5' BELOW GRADE</p> <p>1. PK NAIL FOUND IN PWP - 20.81' 2. PK NAIL FOUND IN PWP - 86.80' 3. PK NAIL FOUND IN PWP - 19.02' 4. MAG SPIKE FOUND IN ROAD - 67.46'</p> <p>SE CORNER, SEC 12, T20N, R9E</p>
<p>22 NORTH: 164915.1530 EAST: 770133.4680</p> <p>FOUND RAILROAD SPIKE, 5' BELOW GRADE</p> <p>1. MAG NAIL FOUND IN PWP, 1' ABOVE GRADE - 14.80' 2. MAG NAIL FOUND IN PWP, 1' ABOVE GRADE - 15.75' 3. MAG NAIL FOUND IN PWP #7102 DETAIL, 1' ABOVE GRADE - 44.70' 4. MAG NAIL FOUND IN PWP, 1' ABOVE GRADE - 41.40'</p> <p>SW CORNER, SEC 13, T20N, R9E</p>	<p>500 NORTH: 169931.7590 EAST: 770148.5393</p> <p>MAG NAIL W/ NPES WASHER SET FLUSH</p> <p>1. 5/8" REBAR WINCAPS CONTROL CAP FLUSH - 257.80' 2. MAG NAIL SET IN W SIDE PWP, 1' ABOVE GRADE - 39.35' 3. E CORNER OF BLOCK PILLAR ON TRACK 10+00 - 68.55'</p> <p>LINE "A" STA. 10+00</p>	<p>501 NORTH: 170931.7576 EAST: 770148.0523</p> <p>MAG NAIL W/ NPES WASHER SET FLUSH</p> <p>1. 5/8" REBAR WINCAPS CONTROL CAP FLUSH - 52.87' 2. MAG NAIL SET IN W SIDE PWP, 4' ABOVE GRADE - 63.40' 3. BENCH SPIKE IN W SIDE PWP, 1' ABOVE GRADE (TBM #2) - 266.31'</p> <p>LINE "A" STA. 20+00</p>	<p>502 NORTH: 171331.7574 EAST: 770148.4702</p> <p>MAG NAIL W/ NPES WASHER SET FLUSH</p> <p>1. BENCH SPIKE IN W SIDE PWP, 1' ABOVE GRADE (TBM #2) - 136.89' 2. NW CORNER OF HOUSE #401 N REIRO RD - 86.17' 3. NE CORNER OF HOUSE #401 N REIRO RD - 107.13'</p> <p>LINE "A" STA. 24+00</p>

[illegible]

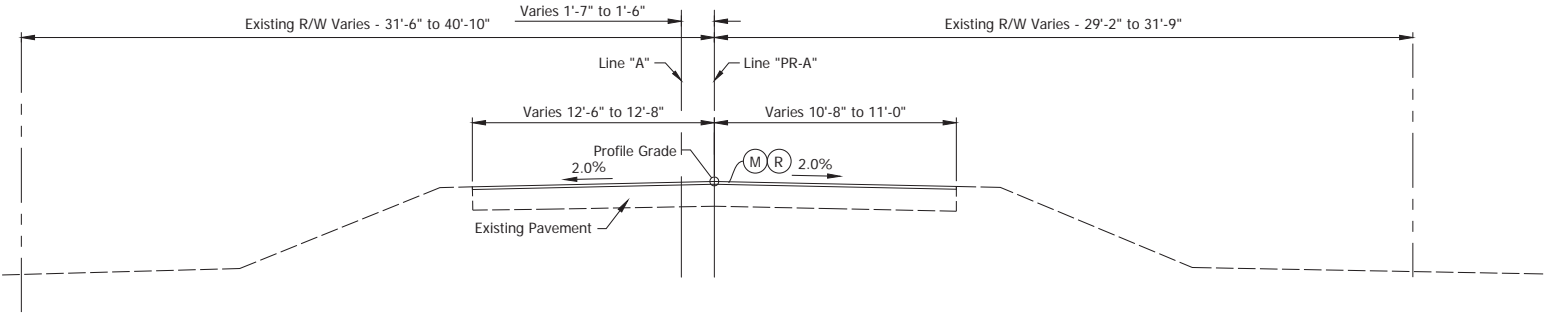
COORDINATE LISTING: CONTROL POINTS									
NUMBER	NORTHING	EASTING	STATION	OFFSET LINE	DESCRIPTION				
4	172901.4053	770150.1100	59+69.65	0.00 R	A 1" Axle in a monument box found 3" below grade				
5	171787.5050	767428.8474	N/A		Manishp capped rebar found flush				
6	172861.0940	767428.8500	N/A		% nail found 1" below grade				
7	172827.5760	764835.6700	N/A		Railroad spike found 4" below grade				
8	170183.6790	764844.0210	N/A		Magnail found 6" below grade				
9	167530.0430	764826.0610	N/A		Railroad spike found 6" below grade				
10	170231.7580	770147.3210	13+00.00	0.00 L	A 1.5" diameter iron pin found flush				
11	168031.0070	770131.7690	N/A		% nail found 1" below grade				
12	172874.1590	772813.5100	N/A		Ashton capped rebar found 5" below grade				
15	173864.5280	755470.7580	N/A		3/4" Diameter iron pin found below grade				
16	175032.9160	775471.9170	N/A		3/4" Diameter iron pin found in monument box				
17	173505.1670	775471.8020	N/A		3/4" Diameter iron pin found in monument box				
18	172913.2160	775462.9560	N/A		3/4" Diameter iron pin found flush				
19	170147.9120	775476.0370	N/A		Magnail found 6" below grade				
20	170215.4180	775476.2980	N/A		Magnail found 6" below grade				
22	164941.1530	770133.0680	N/A		% nail found 5" below grade				
300	170186.4280	770106.0380	12+54.56	41.11 L	A 5/8" Rebar with cap stamped "NPES Traverse Point" set flush				
301	170514.7490	770161.9670	16+82.53	16.24 R	A 5/8" Rebar with cap stamped "NPES Traverse Point" set flush				
302	170582.2610	770167.6400	19+50.91	19.64 R	A 5/8" Rebar with cap stamped "NPES Traverse Point" set flush				
500	169931.7590	770146.5300	10+00.00	0.00 R	A Magnail w/ washer stamped "NORTHPOINT 20100075" set flush				
501	169931.7576	770148.0570	20+00.00	0.00 R	A Magnail w/ washer stamped "NORTHPOINT 20100076" set flush				
502	171331.7374	770148.4702	24+00.00	0.00 R	A Magnail w/ washer stamped "NORTHPOINT 20100075" set flush				
800	172715.7100	775472.1090	N/A		Computed Location - See Report				
801	170701.8794	767493.8006	N/A		Computed Location - See Report				
802	170189.8350	772811.6790	N/A		Computed Location - See Report				
10901	171297.1460	770173.5790	23+65.41	25.14 R	A Miller capped rebar found 4" above grade				
10902	171277.3510	770169.1070	23+45.55	39.21 L	A Ashton capped rebar found 4" below grade				
10903	171501.5670	770108.7170	25+70.11	39.93 L	A Ashton capped rebar found 4" below grade				
10904	171479.6590	769940.5620	N/A		5/8" Diameter rebar found 4" below grade				
10905	171366.3790	769949.5460	N/A		5/8" Diameter rebar found 4" below grade				
10906	171277.0720	769949.1850	N/A		5/8" Diameter rebar found 4" below grade				
10907	171180.5480	769949.3090	N/A		Ashton capped rebar found 4" below grade				
30005	169931.4180	770176.6160	9+99.74	30.08 R	A 5/8" Diameter rebar found 4" below grade				
30006	169919.0340	770174.7730	287.35	30.09 R	A 5/8" Diameter rebar found 6" below grade				
30007	168902.9740	770173.9000	N/A		5/8" Diameter rebar found 4" below grade				

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: SRR	DRAWN: NCG	
CHECKED: JHP	CHECKED: JHP	

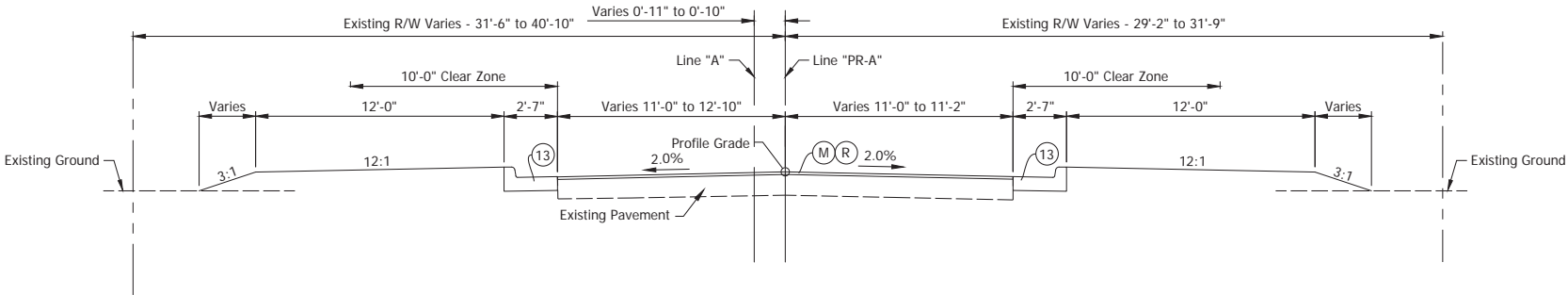
INDEX

Des. No. 1700680

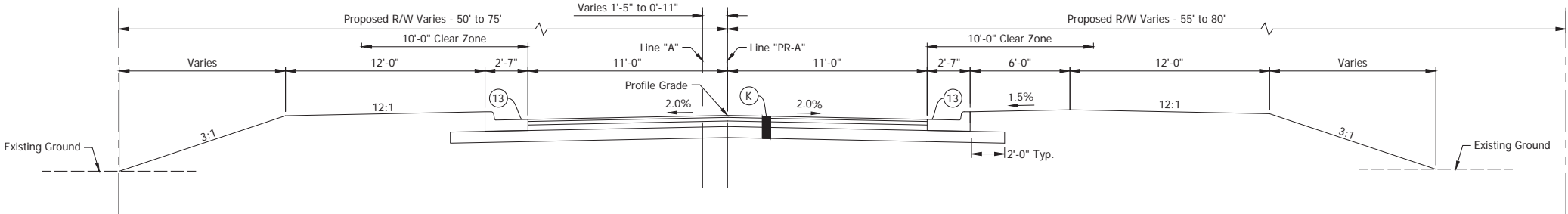
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File: S:_2018\218-0331 Bridge\CD\Plans\Sht_Typ_Sec.dwg



TYPICAL INCIDENTAL CONSTRUCTION
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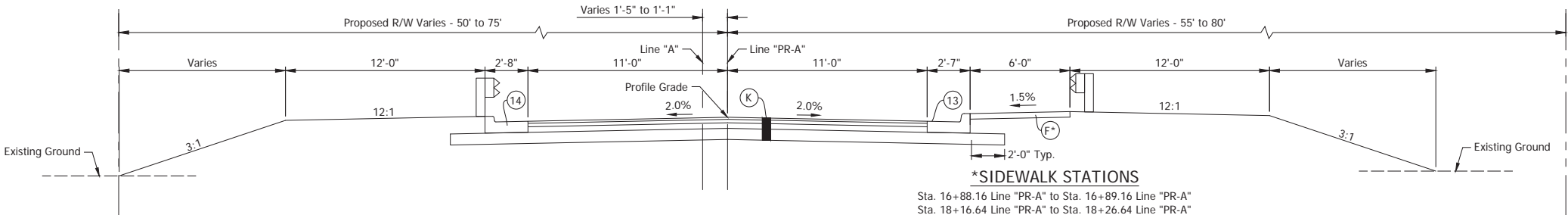
TYPICAL INCIDENTAL CONSTRUCTION
Sta. 22+00.00 Line "PR-A" to Sta. 22+50.00 Line "PR-A"



TYPICAL SECTION

Sta. 14+50.00 Line "PR-A" to Sta. 16+25.58 Line "PR-A"
Sta. 19+47.08 Line "PR-A" to Sta. 22+00.00 Line "PR-A"

Sta. 14+50.00 Line "PR-A" to Sta. 16+02.77 Line "PR-A"
Sta. 18+80.51 Line "PR-A" to Sta. 22+00.00 Line "PR-A"



TYPICAL SECTION

Sta. 16+25.58 Line "PR-A" to Sta. 16+89.16 Line "PR-A"
Sta. 18+16.64 Line "PR-A" to Sta. 19+47.08 Line "PR-A"

Sta. 16+02.77 Line "PR-A" to Sta. 16+89.16 Line "PR-A"
Sta. 18+16.64 Line "PR-A" to Sta. 18+80.51 Line "PR-A"

***SIDEWALK STATIONS**

Sta. 16+88.16 Line "PR-A" to Sta. 16+89.16 Line "PR-A"
Sta. 18+16.64 Line "PR-A" to Sta. 18+26.64 Line "PR-A"

- (K) Pavement Design Will Be Determined After Completion of Geotechnical Report
- (F) Sidewalk, Concrete
- (13) Curb and Gutter, Concrete
- (14) Curb and Gutter, Concrete, Type B

- (M) Milling, Asphalt, 1 1/2 in.
- (R) Pavement Design Will Be Determined After Completion of Geotechnical Report

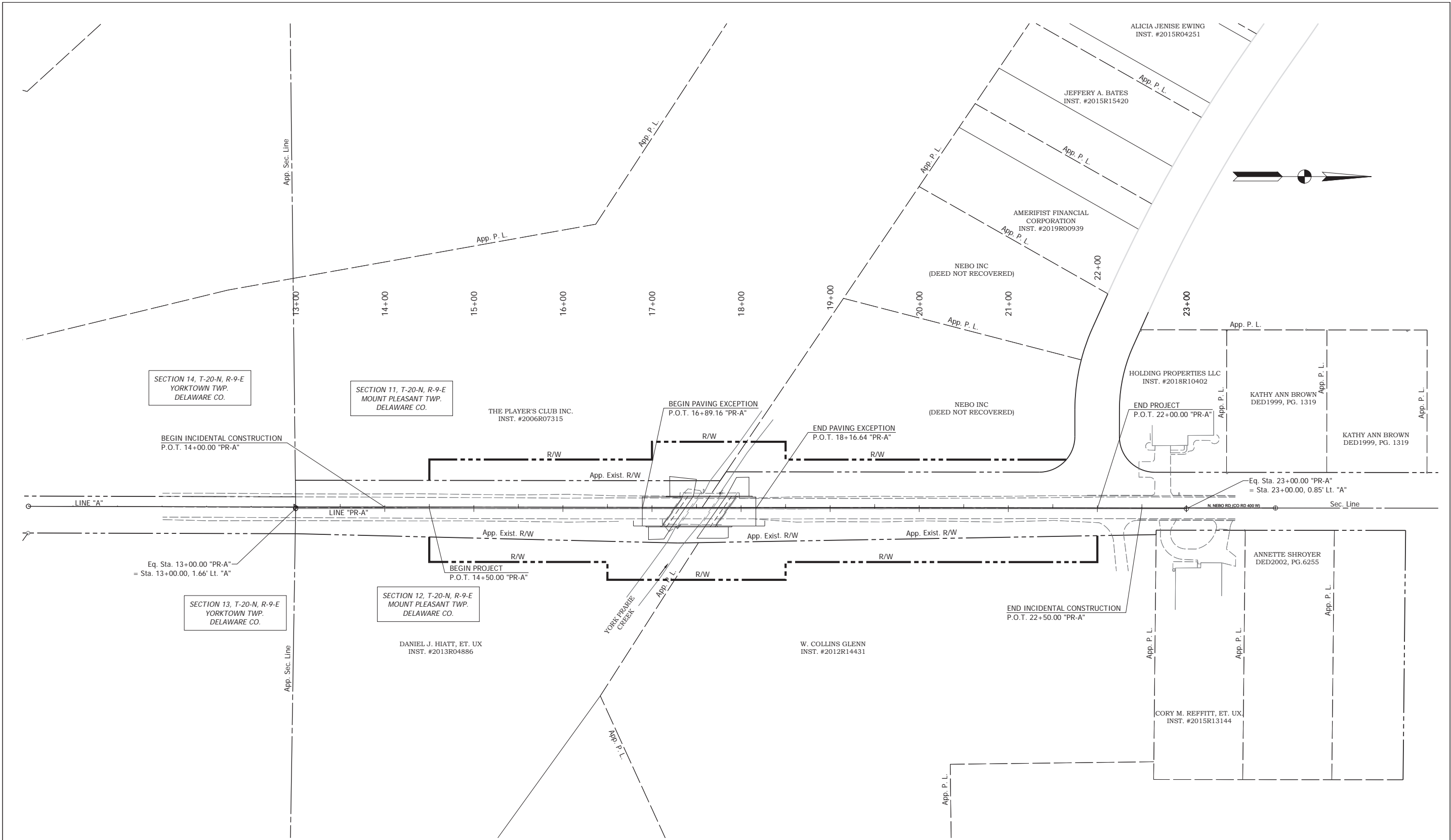
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: MDV		DRAWN: MDV			
CHECKED: BKA		CHECKED: BKA			

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

HORIZONTAL SCALE		BRIDGE FILE	
1/4" = 1'-0"		DELAWARE 00125 B	
VERTICAL SCALE		DESIGNATION	
1/4" = 1'-0"		T700680	
SURVEY BOOK		SHEET	
ELECTRONIC		3 of 17	
CONTRACT		PROJECT	
B-40331		T700680	

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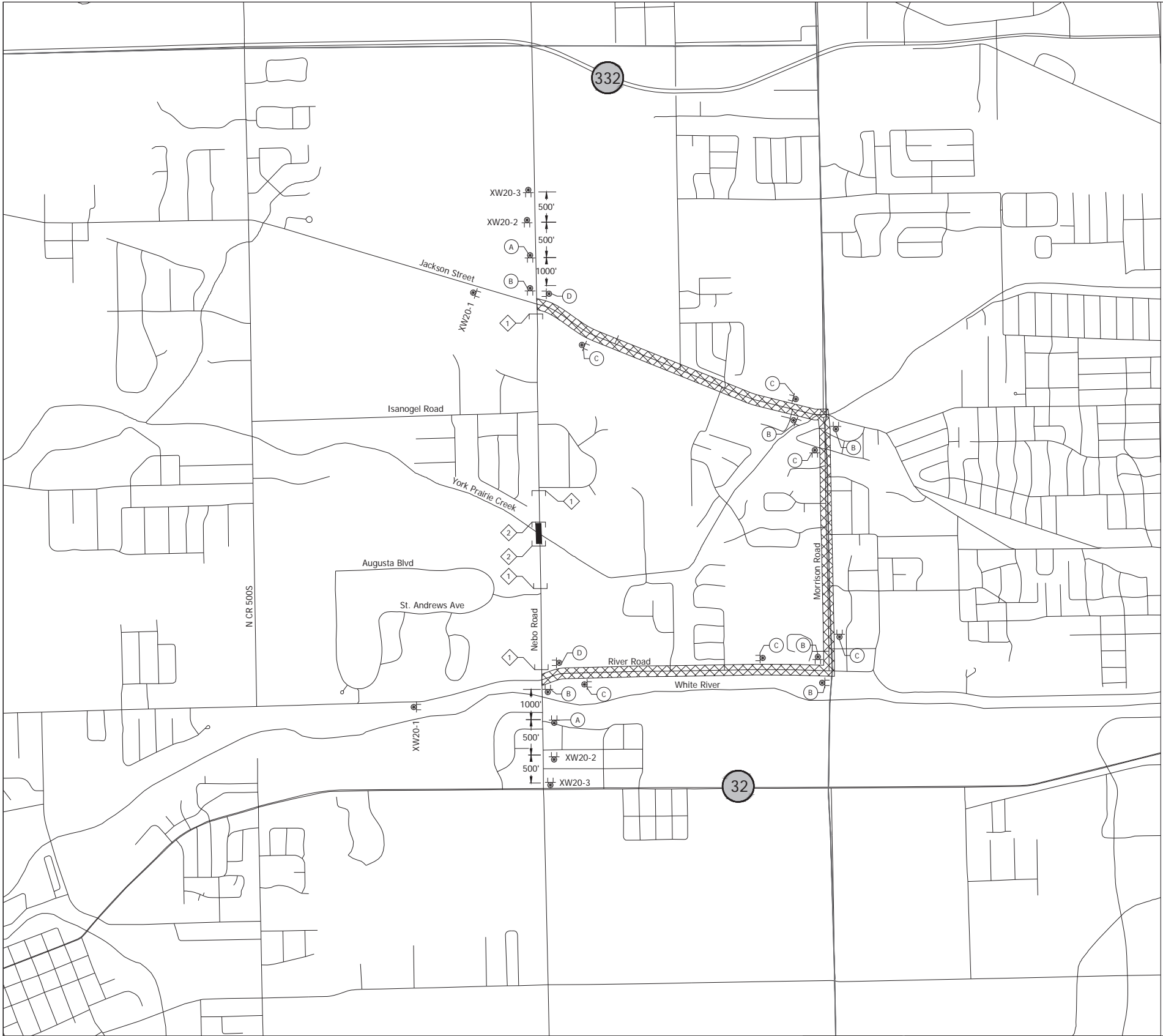


RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____		DATE _____	
DESIGNED: _____ MDV		DRAWN: _____ BS			
CHECKED: _____ BKA		CHECKED: _____ BKA			

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAT NO. 1	

HORIZONTAL SCALE 1"=50'		BRIDGE FILE DELAWARE 00125 B	
VERTICAL SCALE N/A		DESIGNATION 1700680	
SURVEY BOOK		SHEETS 4 of 17	
CONTRACT B-40331		PROJECT 1700680	

Date: May 19, 2020, 3:17pm User Name: Moll
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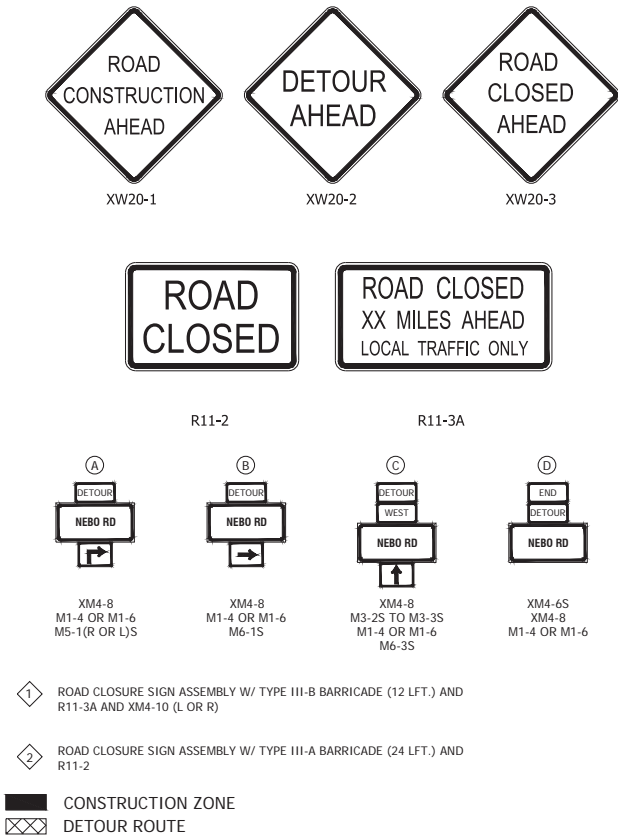


CONSTRUCTION SIGN SCHEDULE				
SIGN NO.	DESCRIPTION	SIZE (IN)	TYPE	QUANTITY
R11-2	"ROAD CLOSED" SIGN	48 x 30	(1)	2
R11-3A	"ROAD CLOSED XX MILES AHEAD" SIGN	60 x 30	(1)	4
XM4-10 (L or R)	"DETOUR" SIGN	48 x 18	(1)	2
XW20-1	"ROAD CONSTRUCTION AHEAD" SIGN	48 x 48	A	2
XW20-2	"DETOUR AHEAD" SIGN	48 x 48	A	2
XW20-3	"ROAD CLOSED AHEAD" SIGN	48 x 48	A	2
TOTAL TYPE "A" SIGNS				6

Detour Route Marker Assemblies: 16 Req'd
Road Closure Sign Assemblies: 6 Req'd
Type III-A Barricades: 48 Lft.
Type III-B Barricades: 48 Lft.

(1) Included with road closure sign assembly.

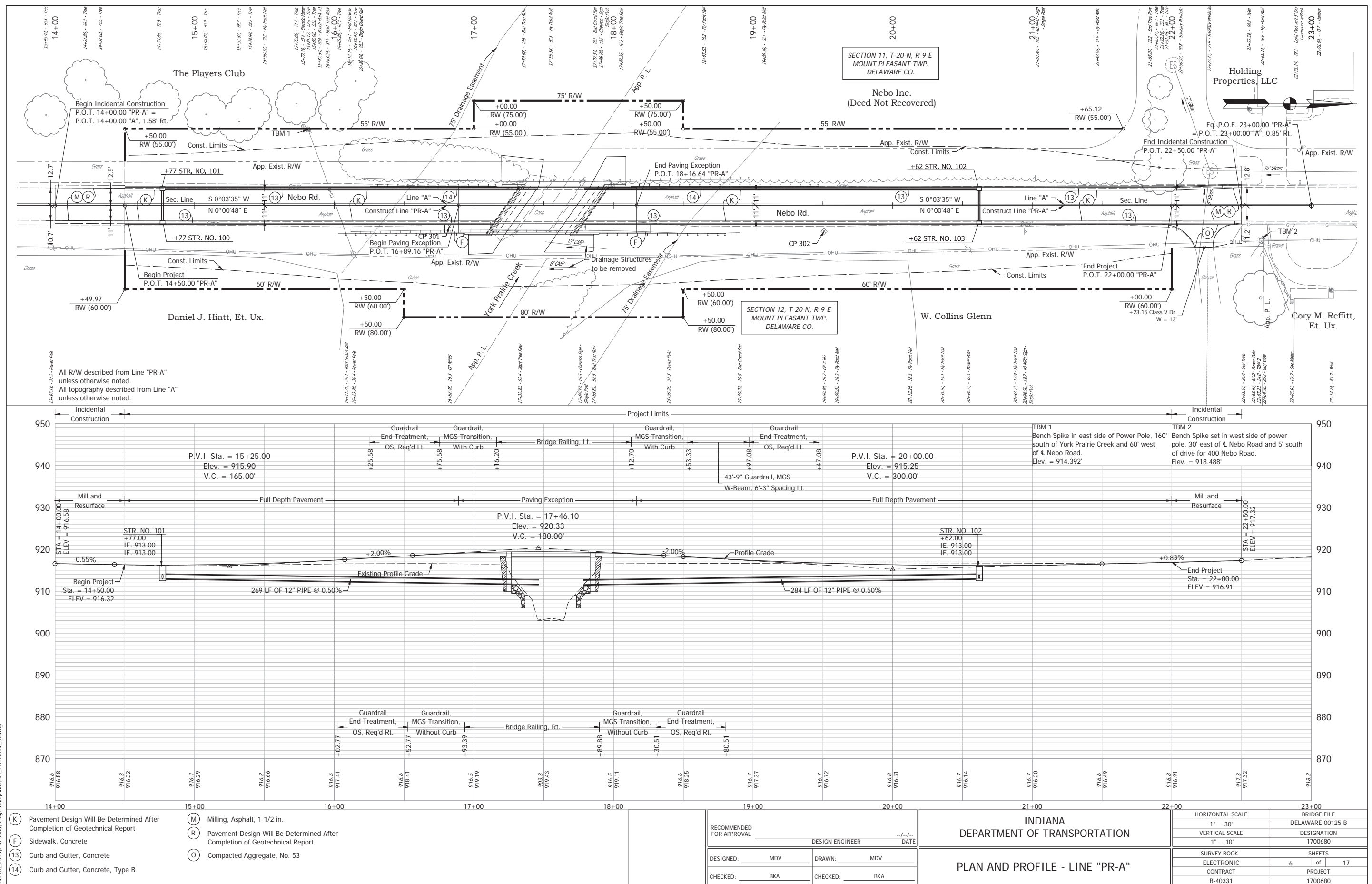
LEGEND

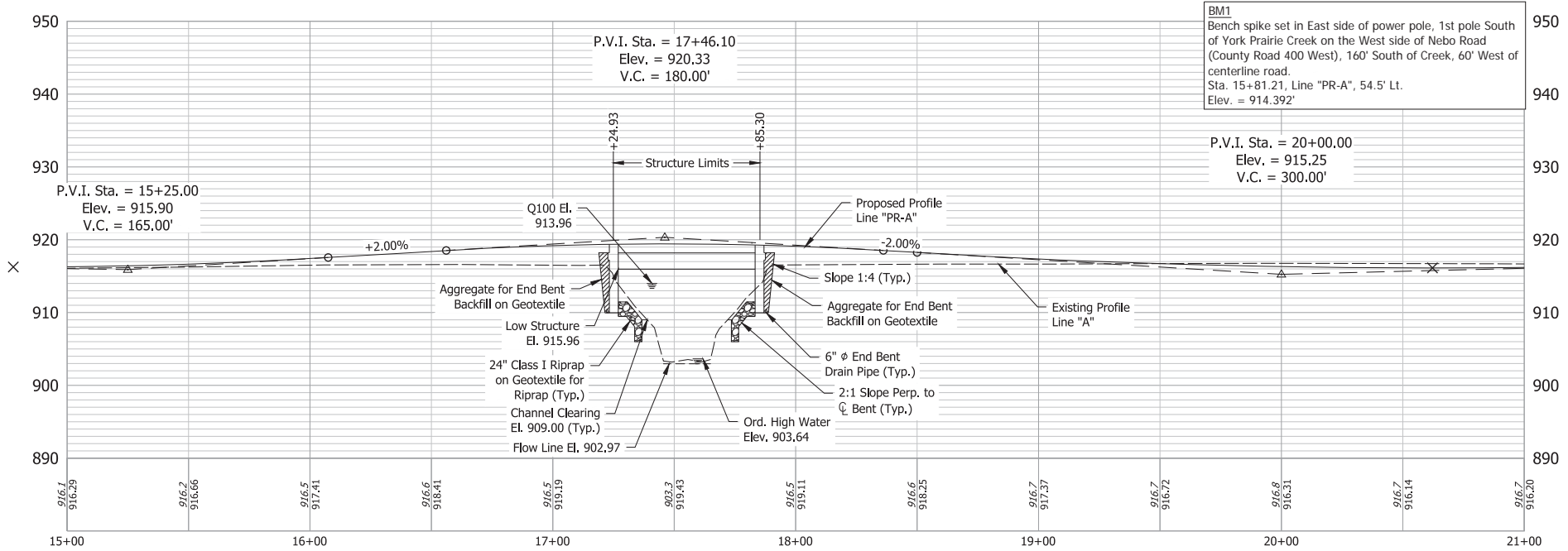
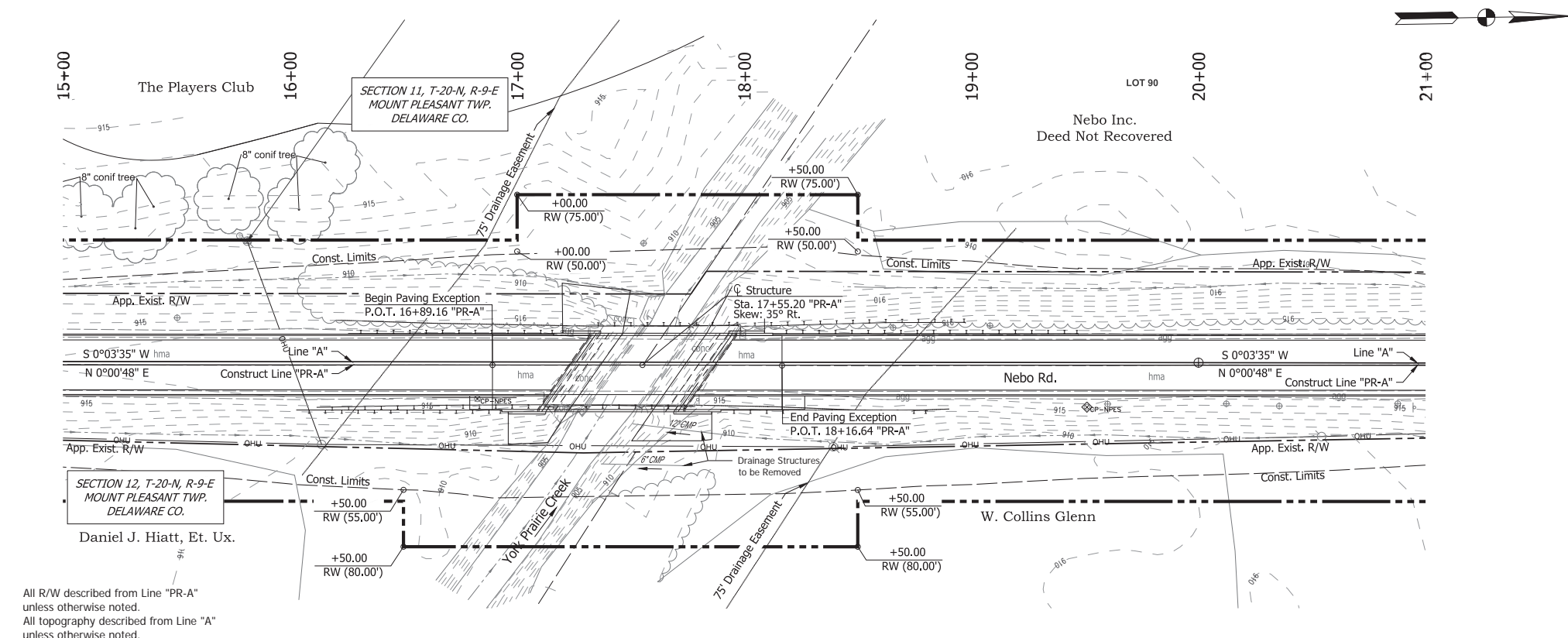


GENERAL NOTES

- All maintenance of traffic devices, signs and pavement markings shall conform to the latest edition of the Indiana MUTCD.
- See INDOT Std. Drwg. 801-TCDT-01 for sign spacing requirements and additional notes.
- See INDOT Std. Drwg. 801-TCLG-01 for standard notes.
- The cost of R11-2, R11-4 and XM4-10 (R or L) shall be included in the cost of the road closure sign assembly.
- Type B construction warning lights shall be used with all signs located on barricades. Type A construction warning lights shall be used on all other construction signs.

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 1000'	BRIDGE FILE DELAWARE 00125 B
DESIGNED: _____ MDV		DRAWN: _____ MDV		VERTICAL SCALE 1" = 1000'	DESIGNATION 1700680
CHECKED: _____ BKA		CHECKED: _____ BKA		SURVEY BOOK	SHEET
				ELECTRONIC	5 of 17
				CONTRACT	PROJECT
				B-40331	1700680





EXISTING STRUCTURE

The existing structure, Delaware 00125, is a three-span continuous reinforced concrete slab bridge built in 1954 and widened in 1975, with a 24' max. span and 34' clear roadway. Existing structure to be removed.

HYDRAULIC DATA

Drainage Area	4.97 SQ. MI.
Q100 Discharge	2,240 CFT./SEC.
Q100 Elevation	913.96 M.S.L.
Q100 Backwater	1.15 FT.
Q100 Velocity	7.33 FT./SEC.
Proposed Waterway Opening, Below Q100	313.48 SFT.
Low Structure Elevation	915.96 M.S.L.
Skew	35°00'00" RT.
Existing Waterway Opening	304.75 SFT.
Existing Low Structure Elevation	914.32 M.S.L.
Existing Backwater	1.73 FT.

HYDRAULIC SCOUR DATA

Q100 Discharge	2,240 CFT./SEC.
Q100 Elevation	913.96 M.S.L.
Q100 Scour Velocity	9.69 FT./SEC.
Q100 Contraction Scour Depth	26.93 FT.
Q100 Total Scour Depth	26.93 FT.
Q100 Low Scour Elevation	876.04 M.S.L.
Q500 Discharge	2,960 CFT./SEC.
Q500 Elevation	914.68 M.S.L.
Q500 Scour Velocity	11.54 FT./SEC.
Q500 Contraction Scour Depth	41.43 FT.
Q500 Total Scour Depth	41.43 FT.
Q500 Low Scour Elevation	861.54 M.S.L.

EARTHWORK TABULATION

Fill	___ cys
Fill + 15%	___ cys
Common Excavation	___ cys
Usable Waterway Excavation	___ cys
Borrow	___ cys
Total Waterway Excavation	___ cys
Excavation Foundation Unclassified	___ cys
Benching (Estimated)	___ cys

No direct payment for Benching. Benching will not be paid for as Common Excavation.

NOTES

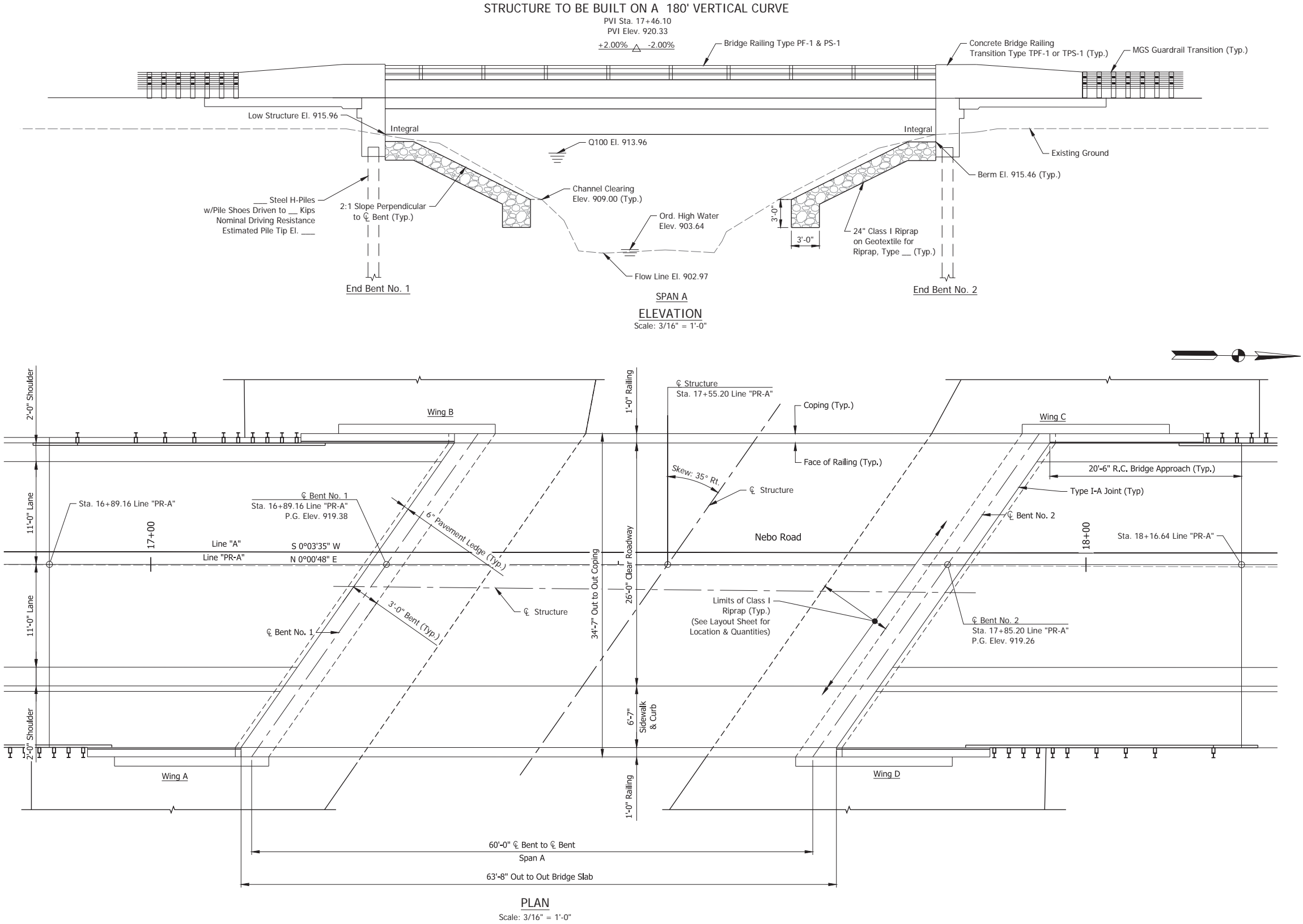
- See Index Sheet for survey reference ties.
- See Plan and Profile Sheet for approach work, incidental construction and additional details.
- M.S.L. = Mean Sea Level.
- See Sheet 2 for utility owners.

COMPOSITE PRESTRESSED CONCRETE
BOX BEAM BRIDGE
1 SPAN: 60'-0"
26'-0" CLEAR ROADWAY; 35° SKEW RT.
NEBO ROAD OVER
YORK PRAIRIE CREEK
DELAWARE COUNTY

Date: May 19, 2020, 3:18pm User Name: MVoil
File: S:_2016\216-0363\Bridg\CAD\Misc\Draws\Layout.dwg

RECOMMENDED FOR APPROVAL _____		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 30'		BRIDGE FILE DELAWARE 00125 B	
DESIGNED: _____ SRR		DRAWN: _____ NCG		VERTICAL SCALE 1" = 10'		DESIGNATION 1700680	
CHECKED: _____ JHP		CHECKED: _____ JHP		LAYOUT		SURVEY BOOK	
						ELECTRONIC	
						CONTRACT	
						B-40331	

Date: May 19, 2020, 3:21pm User Name: M\oll
File: S:_2018\218-0303 Bridge\CD\Plans\General Plan 21803030BD.dwg



COMPOSITE PRESTRESSED CONCRETE
BOX BEAM BRIDGE
1 SPAN: 60'-0"
26'-0" CLEAR ROADWAY; 35° SKEW RT.
NEBO ROAD OVER
YORK PRAIRIE CREEK
DELAWARE COUNTY

RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____		DATE _____	
DESIGNED: _____ SRR		DRAWN: _____ NCG			
CHECKED: _____ JHP		CHECKED: _____ JHP			

INDIANA
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN

HORIZONTAL SCALE		BRIDGE FILE	
3/16" = 1'-0"		DELAWARE 00125 B	
VERTICAL SCALE		DESIGNATION	
3/16" = 1'-0"		1700680	
SURVEY BOOK		SHEET	
ELECTRONIC		8 of 17	
CONTRACT		PROJECT	
B-40331		1700680	

GENERAL NOTES

Reinforcing steel cover shall be 2 1/2" in top and 1" minimum in bottom of floor slab, 3" in footings, except bottom steel which shall be 4", and 2" in all other parts, unless noted.

DESIGN DATA

Designed for HL-93 loading, in accordance with AASHTO LRFD Bridge Design Specifications, Eighth Edition, 2017, and subsequent interims.

DEAD LOAD

Actual weight plus 35 lb/ft² for future wearing surface and 15 lb/ft² for permanent metal deck forms.

FLOOR SLAB

Designed with a 7 1/2" structural depth plus 1/2" sacrificial wearing surface.

DESIGN STRESSES

CONCRETE

Class C f'c = 4000 psi
Class B f'c = 3000 psi
Class A f'c = 3500 psi

REINFORCING STEEL

Grade 60 f'y = 60,000 psi

CONSTRUCTION LOADING

The exterior girder has been checked for strength, deflection, and overturning using the construction loads shown below. Cantilever overhang brackets were assumed for support of the deck overhang past the edge of the exterior girder. The finishing machine was assumed to be supported 6 in. outside the vertical coping form. The top overhang brackets were assumed to be located 6 in. past the edge of the vertical coping form. The bottom overhang brackets were assumed to be braced against the intersection of the girder bottom flange and web.

DECK FALSEWORK LOADS

Designed for 15 lb/ft² for permanent metal stay-in-place deck forms, removable deck forms, and 2-ft exterior walkway.

CONSTRUCTION LIVE LOAD

Designed for 20 lb/ft² extending 2-ft past the edge of coping and 75 lb/ft vertical force applied at a distance of 6 in. outside the face of coping over a 30-ft length of the deck centered with the finishing machine.

FINISHING-MACHINE LOAD

4500 lb distributed over 10 ft along the coping.

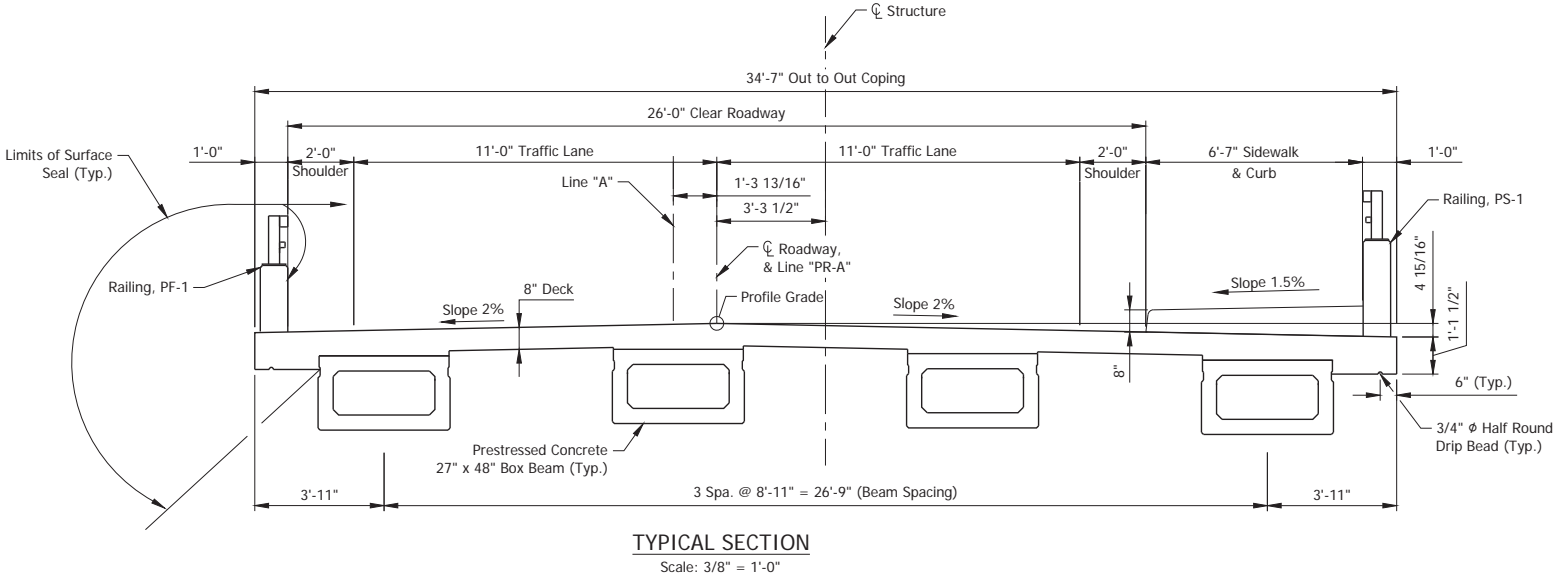
WIND LOAD

Designed for 70 mph horizontal wind loading in accordance with LRFD 3.8.1.

SEISMIC DESIGN LOAD

Seismic Design Category x
Acceleration Coefficient xx
Seismic Soil Profile Type Class x

COMPOSITE PRESTRESSED CONCRETE
BOX BEAM BRIDGE
1 SPAN: 60'-0"
26'-0" CLEAR ROADWAY; 35° SKEW RT.
NEBO ROAD OVER
YORK PRAIRIE CREEK
DELAWARE COUNTY



RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE ____/____/____	
DESIGNED: _____ SRR	DRAWN: _____ NCG
CHECKED: _____ JHP	CHECKED: _____ JHP

INDIANA
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN

HORIZONTAL SCALE 3/8" = 1'-0"		BRIDGE FILE DELAWARE 00125 B	
VERTICAL SCALE 3/8" = 1'-0"		DESIGNATION 1700680	
SURVEY BOOK ELECTRONIC		SHEET 9 of 17	
CONTRACT B-40331		PROJECT 1700680	

Categorical Exclusion

Appendix C

Early Coordination



February 5, 2020

Sample Early
Coordination Letter

Re: Des. No.: 1700680
Bridge No. 125 – Bridge Project
Nebo Road over York Prairie Creek, 0.5 Mile North of County Road (CR) 50 South
Yorktown, Delaware County, Indiana

Note: CR 50 S is also referred to
as River Road

Dear :

The Delaware County Board of Commissioners and the Federal Highway Administration (FHWA) intend to proceed with a project involving Bridge No. 125, which carries Nebo Road over York Prairie Creek in the Town of Yorktown, Delaware County (Des. No. 1700680). This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above Des. No. and project description in your reply.** We will incorporate your comments into the study of the project's environmental impacts.

Project Location and Existing Conditions

The proposed project is located along Nebo Road, approximately 0.5 mile north of CR 50 South in Yorktown, Delaware County, Indiana. Specifically, the project is located in Sections 11 and 12, Township 20 North, Range 9 East in Mt. Pleasant Township as depicted on the Muncie West U. S. Geological Survey 7.5-minute quadrangle. Adjacent land use consists of residential and commercial (private golf course) properties and agricultural fields. Please see attachments for maps and photographs of the proposed project area.

Nebo Road is functionally classified as an urban minor arterial within the project area and consists of two, 11-foot asphalt travel lanes (one in each direction) with a 1-foot paved shoulder and a variable width gravel shoulder on both sides. The posted speed limit within the project area is 40 miles per hour. Bridge No. 125 is a 67-foot long concrete continuous slab bridge widened with steel beams with a 34.6-foot out-to-out width that carries Nebo Road over York Prairie Creek. The structure was built in 1954 and reconstructed in 1975. The typical section of Nebo Road over the existing bridge consists of two 11-foot travel lanes with a 5-foot shoulder on both sides.

Purpose and Need

The need for the project stems from the deteriorated condition of the structure. During the routine inspection on November 9, 2018, a gap was noted between the bridge and the copings on both the east and west sides of the bridge. Additionally, a large diagonal crack was observed in the northwest corner and spalling was observed under the bridge where the beams meet the pier on both the north and south abutment of the bridge. Vertical cracks at both the north and

3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268
PHONE: 317.222.3878 • TOLL FREE: 800.423.7422

south abutments and general rusting and deterioration of the steel bridge beams were also noted. Erosion of the bank of York Prairie Creek is undermining the slopewalls and causing sections to break off. The deck, wearing surface, and stream channel all had condition ratings of 5, which is considered “fair.” The superstructure had a condition rating of 4, which is considered “poor.” The substructure had a condition rating of 3 which is considered “serious.” Additionally, according to the Hydraulic Report prepared by Lochmueller Group in October 2019, the current low structure elevation (914.32 feet) is less than two feet greater than the 100-year floodplain elevation (913.96 feet) of York Prairie Creek. This is considered to be hydraulically deficient. The purpose of the project is to extend the service life of this crossing. Any improvements made to the crossing should bring the condition ratings of all of the structure components to at least 8, which is considered very good condition. The secondary purpose of the project is to provide a structure that is more hydraulically sufficient.

Proposed Project

The proposed project will replace the existing bridge with a single-span, composite, prestressed box beam bridge with a 60-foot span. The roadway will be raised 2.5 feet across the bridge to provide adequate hydraulics for the new structure. The new cross-section of Nebo Road over the bridge will be two 11-foot travel lanes with 2-foot gutters on both sides. Additionally, a 6-foot sidewalk with a 7-inch curb will be constructed along the east side of the bridge and along the east side of Nebo Road for a total length of 767. The new out-to-out coping width will be 34-foot, 7-inches with a 22-foot clear roadway width. The existing guardrail along both sides of Nebo Road will be replaced with new standard guardrail. New riprap will be placed along the spill slopes. The approach roadway, from approximately 413 feet north and 281 feet south of the structure, will be replaced to full depth and will be gently raised to accommodate the 2.5-foot raise in profile at the bridge. Additionally, 42.5 feet south and 50 feet north of the full-depth approach replacement will have 1.5 inches of its surface milled and 1.5 inches of hot mix asphalt overlay applied. The length of channel work along York Prairie Creek will be approximately 96.5 feet. The total project length along Nebo Road is 850 feet.

The maintenance of traffic (MOT) plan will include the closure of Nebo Road within the project area. A detour utilizing Jackson Street, Morrison Road, and W River Road will be established. Signs, barrels, and flashing signals will be utilized along Nebo Road to notify motorists of the road closure. Access will be maintained for property owners during construction of the project. The MOT will be implemented per the current *Indiana Design Manual* guidelines.

Construction is anticipated to begin in Fiscal Year (FY) 2022.

Right-of-Way (ROW)

Existing ROW along Nebo Road varies from 31 to 40 feet from the centerline on both sides of the roadway of Nebo Road. The proposed project will require the acquisition of approximately 0.9 acre of permanent ROW. The proposed ROW will widen to a typical width of 50 feet wide to the west and 55 feet wide to the east of the Nebo Road centerline. The anticipated maximum ROW width would be approximately 75 feet to the west and 80 feet to the east of the Nebo Road centerline. Minor tree clearing will be required for construction access and grading.

Environmental Resources

A Red Flag Investigation (RFI) was performed for a 0.5-mile radius for the project area. Several “Red Flags” were identified within the 0.5-mile search radius; however, not all will impact the proposed project. The Player’s Club Golf Course is located adjacent to the project area. York Prairie Creek flows through the project area and the project area is within a regulated floodplain. Due to the proximity of water resources in the project area, a *Waters of the U.S. Determination Report* will be completed as a part of the project. This project is outside the Karst Memorandum of Understanding Potential Karst Features Region.

Lochmueller Group conducted a field investigation of the project area on July 31, 2019. The field investigation identified York Prairie Creek within the project area.

Urbanized Area Boundary (UAB)

The project lies within the Yorktown UAB, and in accordance with 327 IAC 15-13 (Rule 13 – Municipal Separate Storm Sewer Systems), Delaware County will develop a Storm Water Quality Management Plan. As part of its implementation, projects falling within the UAB will be required to consider appropriate post-construction storm water quality best management practices (BMPs). These BMPs should take into consideration the available space, pollutants of concern, and receiving waters.

Section 106

The National Register of Historic Places (National Register) and the Indiana Register of Historic Sites and Structures (State Register) were reviewed using the State Historic Architectural and Archaeological Research Database (SHAARD) and SHAARD Geographic Information System (GIS) data published online. No above-ground resources on either list are within the project area. The 1985 *Delaware County Interim Report: Indiana Historic Sites and Structures Inventory* (IHSSI) data was also examined; no surveyed resources from this inventory were located within the project area. The *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* by Mead & Hunt (2009) was reviewed. No bridges eligible for listing in the National Register are within the project area. No cemeteries were noted within the vicinity of the project area. It is anticipated that this project will qualify for the Minor Projects Programmatic Agreement (MPPA).

Range-wide Informal Programmatic Consultation

Delaware County is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). The U.S. Fish and Wildlife Service (USFWS) Range-wide Programmatic Informal Consultation for the Indiana bat and NLEB will be completed for this project. Completion of the appropriate determination key through the USFWS Information for Planning and Consultation (IPaC) portal will occur. If a likely determination of “Not Likely to Adversely Affect,” is reached then additional consultation with the USFWS will occur through INDOT.

Early Coordination

Should we not receive your response within 30 calendar days from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request.

If you have any questions regarding this project, please feel free to contact me at (317) 222-3880 or at ckunkel@lochgroup.com. Additionally, should you want to contact the sponsor of this project, Delaware County, please contact the Project Director, Angela Moyer at (765) 747-7765 or at amoyer@co.delaware.in.us

Thank you in advance for your input.

Sincerely,



Chris Kunkel
Environmental Specialist
Lochmueller Group, Inc.

Attachments:

- General Location Map
- USGS Topographic Map
- Aerial Map (2017)
- Red Flag Investigation Maps
- Photographs

Removed to avoid duplication; see Appendix B and Appendix E

Distribution List:

- USFWS, Bloomington Field Office (electronic submission)
- Natural Resources Conservation Service, Indianapolis Office (electronic submission)
- U.S. Army Corps of Engineers, Louisville District (electronic submission)
- U.S. Housing and Urban Development (electronic submission)
- National Park Service
- FHWA – Indiana Division (electronic submission)
- IDNR, Division of Fish and Wildlife (electronic submission)
- IDEM (electronic submission)
- INDOT, Office of Public Involvement (electronic submission)
- INDOT, Environmental Services (electronic submission)
- INDOT, Greenfield District (electronic submission)
- Indiana Geological Survey (electronic submission)
- Delaware County Board of Commissioners

- Delaware County Council
- Delaware County Highway Department (electronic submission)
- Delaware County Drainage Board
- Delaware County Surveyor's Office (electronic submission)
- Delaware County Emergency Management Agency (electronic submission)
- Delaware County Emergency Medical Service (electronic submission)
- Delaware County Sheriff's Department (electronic submission)
- Yorktown Parks Department
- Yorktown/Mt. Pleasant Township Committee
- Yorktown Street Department
- Yorktown Town Council
- Yorktown Police Department
- Yorktown Community Schools
- Yorktown Fire Department
- Delaware-Muncie Metropolitan Plan Commission
- The Player's Club Golf Course

Organization and Project Information

Project ID:
Des. ID: 1700680
Project Title: Delaware 125 Bridge Project
Name of Organization: Lochmueller Group
Requested by: Chris Kunkel

Environmental Assessment Report

1. Geological Hazards:
 - Moderate liquefaction potential
 - Floodway
2. Mineral Resources:
 - Bedrock Resource: High Potential
 - Sand and Gravel Resource: High Potential
3. Active or abandoned mineral resources extraction sites:
 - Abandoned Industrial Minerals Sand Gravel Pits

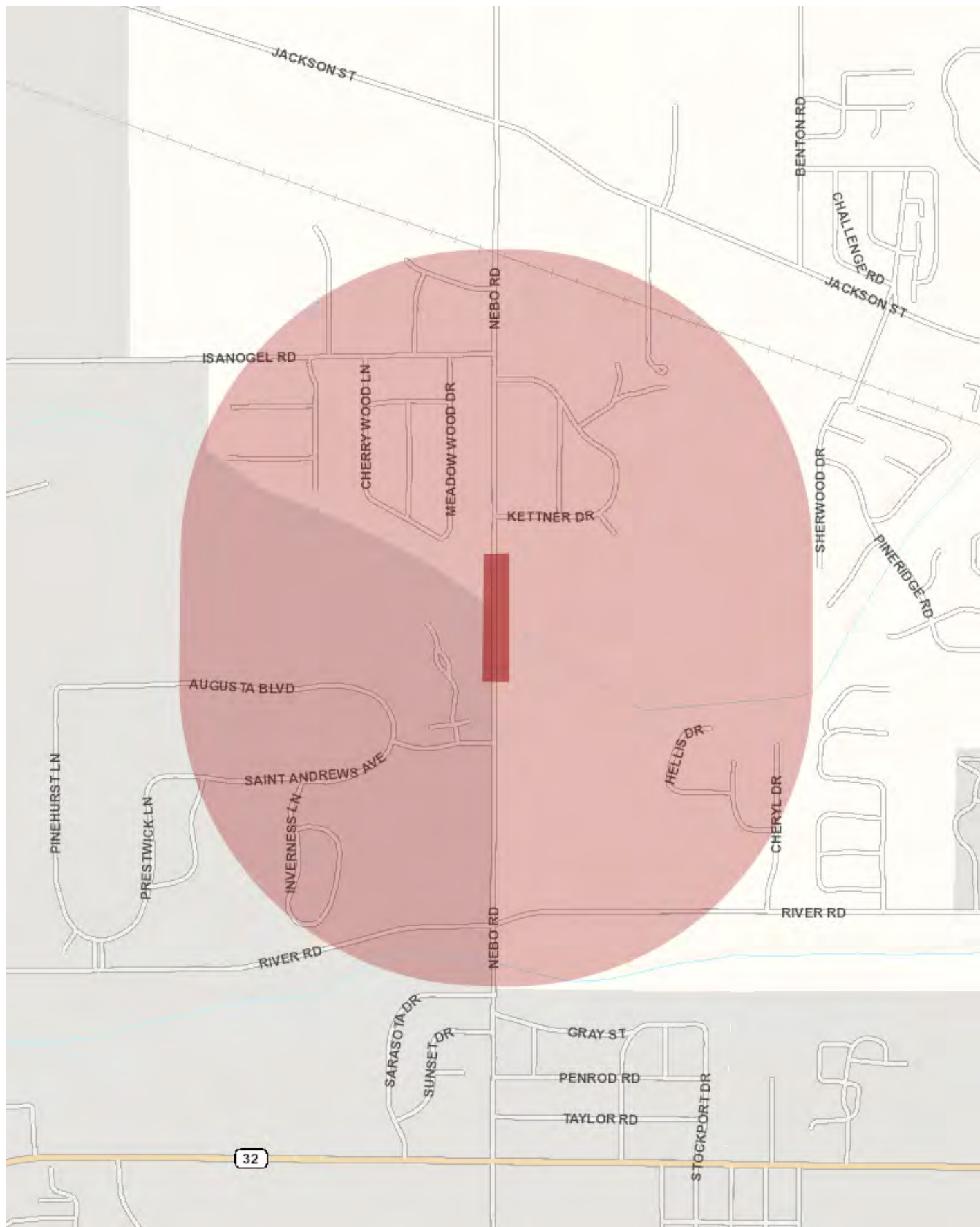
*All map layers from Indiana Map (maps.indiana.edu)

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This information was furnished by Indiana Geological Survey
Address: 420 N. Walnut St., Bloomington, IN 47404
Email: IGSEnvir@indiana.edu
Phone: 812 855-7428

Date: February 05, 2020



Metadata:

- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Pits_Abandoned.html
- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

Delaware County
Angela Moyer
7700 East Jackson Street
Muncie , IN 47302

Lochmueller Group
Chris Kunkel
3502 Woodview Trace
Suite 150
Indianapolis , IN 46268

Date

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: The Delaware County Board of Commissioners and the Federal Highway Administration (FHWA) intend to proceed with a project involving Bridge No. 125, which carries Nebo Road over York Prairie Creek in the Town of Yorktown, Delaware County (Des. No. 1700680). The proposed project is located along Nebo Road, approximately 0.5 mile north of CR 50 South in Yorktown, Delaware County, Indiana. Specifically, the project is located in Sections 11 and 12, Township 20 North, Range 9 East in Mt. Pleasant Township as depicted on the Muncie West U. S. Geological Survey 7.5-minute quadrangle. Adjacent land use consists of residential and commercial (private golf course) properties and agricultural fields. The proposed project will replace the existing bridge with a single-span, composite, prestressed box beam bridge with a 60-foot span. The roadway will be raised 2.5 feet across the bridge to provide adequate hydraulics for the new structure. The new cross-section of Nebo Road over the bridge will be two 11-foot travel lanes with 2-foot gutters on both sides. Additionally, a 6-foot sidewalk with a 7-inch curb will be constructed along the east side of the bridge and along the east side of Nebo Road for a total length of 767. The new out-to-out coping width will be 34-foot, 7-inches with a 22-foot clear roadway width. The existing guardrail along both sides of Nebo Road will be replaced with new standard guardrail. New riprap will be placed along the spill slopes. The approach roadway, from approximately 413 feet north and 281 feet south of the structure, will be replaced to full depth and will be gently raised to accommodate the 2.5-foot raise in profile at the bridge. Additionally, 42.5 feet south and 50 feet north of the full-depth approach replacement will have 1.5 inches of its surface milled and 1.5 inches of hot mix asphalt overlay applied. The length of channel work along York Prairie Creek will be approximately 96.5 feet. The total project length along Nebo Road is 850 feet. The maintenance of traffic (MOT) plan will include the closure of Nebo Road within the project area. A detour utilizing Jackson Street, Morrison Road, and W River Road will be established. Signs, barrels, and flashing signals will be utilized along Nebo Road to notify motorists of the road closure. Access will be maintained for property owners during construction of the project. The MOT will be implemented per the current Indiana Design Manual guidelines. A Red Flag Investigation (RFI) was performed for a 0.5-mile radius for the project area. Several "Red Flags" were identified within the 0.5-mile search radius; however, not all will impact the proposed project. The Player's Club Golf Course is located adjacent to the project area. York Prairie Creek flows through the project area and the project area is within a regulated floodplain. Due to the proximity of water resources in the project area, a Waters of the U.S. Determination Report will be completed as a part of the project. This project is outside the Karst Memorandum of Understanding Potential Karst Features Region. Lochmueller Group conducted a field investigation of the project area on July 31, 2019. The field investigation identified York Prairie Creek within the project area.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal

National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).

3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana . A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
 - IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - IC 14-28-1 Flood Control Act 310 IAC 6-1
 - IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
 - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from

IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at:
<http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of

the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf (http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf).) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all

lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).

5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2 , Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF> (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

The Delaware County Board of Commissioners and the Federal Highway Administration (FHWA) intend to proceed with a project involving Bridge No. 125, which carries Nebo Road over York Prairie Creek in the Town of Yorktown, Delaware County (Des. No. 1700680). The proposed project is located along Nebo Road, approximately 0.5 mile north of CR 50 South in Yorktown, Delaware County, Indiana. Specifically, the project is located in Sections 11 and 12, Township 20 North, Range 9 East in Mt. Pleasant Township as depicted on the Muncie West U. S. Geological Survey 7.5-minute quadrangle. Adjacent land use consists of residential and commercial (private golf course) properties and agricultural fields. The proposed project will replace the existing bridge with a single-span, composite, prestressed box beam bridge with a 60-foot span. The roadway will be raised 2.5 feet across the bridge to provide adequate hydraulics for the new structure. The new cross-section of Nebo Road over the bridge will be two 11-foot travel lanes with 2-foot gutters on both sides. Additionally, a 6-foot sidewalk with a 7-inch curb will be constructed along the east side of the bridge and along the east side of Nebo Road for a total length of 767. The new out-to-out coping width will be 34-foot, 7-inches with a 22-foot clear roadway width. The existing guardrail along both sides of Nebo Road will be replaced with new standard guardrail. New riprap will be placed along the spill slopes. The approach roadway, from approximately 413 feet north and 281 feet south of the structure, will be replaced to full depth and will be gently raised to accommodate the 2.5-foot raise in profile at the bridge. Additionally, 42.5 feet south and 50 feet north of the full-depth approach replacement will have 1.5 inches of its surface milled and 1.5 inches of hot mix asphalt overlay applied. The length of channel work along York Prairie Creek will be approximately 96.5 feet. The total project length along Nebo Road is 850 feet. The maintenance of traffic (MOT) plan will include the closure of Nebo Road within the project area. A detour utilizing Jackson Street, Morrison Road, and W River Road will be established. Signs, barrels, and flashing signals will be utilized along Nebo Road to notify motorists of the road closure. Access will be maintained for property owners during construction of the project. The MOT will be implemented per the current Indiana Design Manual guidelines. A Red Flag Investigation (RFI) was performed for a 0.5-mile radius for the project area. Several "Red Flags" were identified within the 0.5-mile search radius; however, not all will impact the proposed project. The Player's Club Golf Course is located adjacent to the project area. York Prairie Creek flows through the project area and the project area is within a regulated floodplain. Due to the proximity of water resources in the project area, a Waters of the U.S. Determination Report will be completed as a part of the project. This project is outside the Karst Memorandum of Understanding Potential Karst Features Region. Lochmueller Group conducted a field investigation of the project area on July 31, 2019. The field investigation identified York Prairie Creek within the project area.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date:

2/6/2020

Signature of the INDOT

Project Engineer or Other Responsible Agent

Angela Moyer

Angela Moyer

Date: 02/05/2020

Signature of the

For Hire Consultant

Chris Kunkel

Chris Kunkel

February 20, 2020

Chris Kunkel
Lochmueller Group, Inc.
3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268

Dear Mr. Kunkel:

The proposed project to address the deteriorating condition of bridge number 125, along Nebo Road over York Prairie Creek in Yorktown, Delaware County, Indiana, (Des No 1700680), as referred to in your letter received February 5, 2020, will cause a conversion of prime farmland.

The attached packet of information is for your use competing Parts VI and VII of the AD-1006. After completion, the federal funding agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

JERRY RAYNOR
Digitally signed by JERRY RAYNOR
Date: 2020.02.23 19:28:51 -05'00'

JERRY RAYNOR
State Conservationist

Enclosures

Helping People Help the Land.



USDA is an equal opportunity provider, employer and lender.

FARMLAND CONVERSION IMPACT RATING FOR CORRIDOR TYPE PROJECTS

PART I (To be completed by Federal Agency)		3. Date of Land Evaluation Request		4. Sheet 1 of _____	
1. Name of Project		5. Federal Agency Involved			
2. Type of Project		6. County and State			
PART II (To be completed by NRCS)		1. Date Request Received by NRCS		2. Person Completing Form	
3. Does the corridor contain prime, unique statewide or local important farmland? (If no, the FPPA does not apply - Do not complete additional parts of this form).		YES <input type="checkbox"/> NO <input type="checkbox"/>		4. Acres Irrigated Average Farm Size	
5. Major Crop(s)	6. Farmable Land in Government Jurisdiction Acres: _____ %		7. Amount of Farmland As Defined in FPPA Acres: _____ %		
8. Name Of Land Evaluation System Used	9. Name of Local Site Assessment System		10. Date Land Evaluation Returned by NRCS		
PART III (To be completed by Federal Agency)		Alternative Corridor For Segment			
		Corridor A	Corridor B	Corridor C	Corridor D
A. Total Acres To Be Converted Directly					
B. Total Acres To Be Converted Indirectly, Or To Receive Services					
C. Total Acres In Corridor					
PART IV (To be completed by NRCS) Land Evaluation Information					
A. Total Acres Prime And Unique Farmland					
B. Total Acres Statewide And Local Important Farmland					
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted					
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value					
PART V (To be completed by NRCS) Land Evaluation Information Criterion Relative value of Farmland to Be Serviced or Converted (Scale of 0 - 100 Points)					
PART VI (To be completed by Federal Agency) Corridor Assessment Criteria (These criteria are explained in 7 CFR 658.5(c))		Maximum Points			
1. Area in Nonurban Use		15			
2. Perimeter in Nonurban Use		10			
3. Percent Of Corridor Being Farmed		20			
4. Protection Provided By State And Local Government		20			
5. Size of Present Farm Unit Compared To Average		10			
6. Creation Of Nonfarmable Farmland		25			
7. Availability Of Farm Support Services		5			
8. On-Farm Investments		20			
9. Effects Of Conversion On Farm Support Services		25			
10. Compatibility With Existing Agricultural Use		10			
TOTAL CORRIDOR ASSESSMENT POINTS		160			
PART VII (To be completed by Federal Agency)					
Relative Value Of Farmland (From Part V)		100			
Total Corridor Assessment (From Part VI above or a local site assessment)		160			
TOTAL POINTS (Total of above 2 lines)		260			
1. Corridor Selected:	2. Total Acres of Farmlands to be Converted by Project:	3. Date Of Selection:	4. Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input type="checkbox"/>		
5. Reason For Selection:					

Signature of Person Completing this Part:

Chris Kunkel

DATE

NOTE: Complete a form for each segment with more than one Alternate Corridor



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Joe McGuinness, Commissioner

Chris Kunkel
Lochmueller Group, Inc.
(317) 222-3880
ckunkel@lochgroup.com

February 27, 2020

Re: Early Coordination Review, Des. 1700680

Bridge No. 125_Nebo Road over York Prairie Creek in Yorktown, Delaware County

Dear Mr. Kunkel:

The Indiana Department of Transportation (INDOT) Environmental Services Division (ESD) appreciates the opportunity to assist you on the project referenced above. Pursuant to your early coordination request for an environmental review, we have performed a preliminary search of the project area.

There appear to be several active projects you should be aware of that are within roughly 1 mile of your project area. A summary of this project is provided below. Contact information for the project manager (or program manager) is provided if you would like to request additional information.

INDOT Project Manager: Hung Pham; hpham@indot.in.gov; 317-467-3984

Contract: R-36337; DES: 1006111 - Intersection Improvement, Nebo Rd at Jackson St.

Contract: R-34989; DES: 0900596 - Bike/Pedestrian Facilities at River Rd. along White River from Morrison Rd. to CR 500W.

INDOT Project Manager: Michelle Loveall; mloveall@indot.in.gov; 317-467-3438

Contract: R-40329; DES: 1700678 - Intersection Improvement at Nebo Road and River Road, including approaches.

Contract: R-39110; DES: 1702868 - Bike/Pedestrian Facilities along bridge 124, Nebo Rd over White River.

Contract: R-39110; DES: 1592998 - Bike/Pedestrian Facilities at Nebo Road from SR 32 to River Road.

Contract: RS-39995; DES: 1593199 - HMA Overlay, Preventive Maintenance on Route SR 32; From SR 9 N Jct. to US 35.

INDOT Program Manager: Scott Bailey; sbailey14@indot.in.gov; 317-467-3969

Contract: IR-30174; DES: 9700310 - Added Travel Lanes on Route SR 32; From CR 575 W to CR 400 W.

Contract: IR-30174; DES: 0810377 - Other Type Project (Miscellaneous) on Route SR 32; Sidewalk construction from CR 575 W to CR 400 W.

Contract: IR-30174; DES: 0810049 - Utility Relocation on Route SR 32; from CR 575 W to CR 400 W.

Appropriate hazardous materials investigations should be conducted in areas of excavation. If during the Hazardous Material investigation sites are identified that have a reasonable potential to impact the project area(s), ESD recommends that the Indiana Department of Environmental Management's (IDEM) Virtual File Cabinet (VFC) be consulted. The VFC will provide information that is useful in assessing the risk of impacts.

If your project will require the use of state right-of-way, please contact the In-House Services Manager at the INDOT Greenfield District Office.

As always, be sure to follow all applicable processes as well as federal and state laws and local requirements. Thank you for the opportunity to assist you with your project. If you have any questions, please contact a member of my staff, Tomas Beauchamp, tbeauchamp@indot.in.gov, or 317-234-5071.

Sincerely,



Ron Bales
Environmental Policy Manager,
Environmental Services Division



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

March 03, 2020

Consultation Code: 03E12000-2020-SLI-0962

Event Code: 03E12000-2020-E-04381

Project Name: Delaware No. 125 over York Prairie Creek Bridge Replacement Project - Des. No. 1700680

Subject: Updated list of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2020-SLI-0962

Event Code: 03E12000-2020-E-04381

Project Name: Delaware No. 125 over York Prairie Creek Bridge Replacement Project - Des. No. 1700680

Project Type: TRANSPORTATION

Project Description: The Delaware County Board of Commissioners and the Federal Highway Administration (FHWA) intend to proceed with a project involving Bridge No. 125, which carries Nebo Road over York Prairie Creek in the Town of Yorktown, Delaware County (Des. No. 1700680). The proposed project is located along Nebo Road, approximately 0.5 mile north of CR 50 South in Yorktown, Delaware County, Indiana. The proposed project will replace the existing bridge with a single-span, composite, prestressed box beam bridge with a 60-foot span. The roadway will be raised 2.5 feet across the bridge to provide adequate hydraulics for the new structure. The new cross-section of Nebo Road over the bridge will be two 11-foot travel lanes with 2-foot gutters on both sides. Additionally, a 6-foot sidewalk with a 7-inch curb will be constructed along the east side of the bridge and along the east side of Nebo Road for a total length of 767 feet. The new out-to-out coping width will be 34-foot, 7-inches with a 22-foot clear roadway width. The existing guardrail along both sides of Nebo Road will be replaced with new standard guardrail. New riprap will be placed along the spill slopes. The approach roadway, from approximately 413 feet north and 281 feet south of the structure, will be replaced to full depth and will be gently raised to accommodate the 2.5-foot raise in profile at the bridge. Additionally, 42.5 feet south and 50 feet north of the full-depth approach replacement will have 1.5 inches of its surface milled and 1.5 inches of hot mix asphalt overlay applied. The length of channel work along York Prairie Creek will be approximately 96.5 feet. The total project length along Nebo Road is 850 feet. The maintenance of traffic (MOT) plan will include the closure of Nebo Road within the project area. A detour utilizing Jackson Street, Morrison Road, and W River Road will be established. Signs, barrels, and flashing signals will be utilized along Nebo Road to notify motorists of the road closure. Access will be maintained for property owners during construction of the project. The MOT will be implemented per the current Indiana Design Manual guidelines. Suitable summer bat habitat exists within and near the project area. Approximately 0.34 acre of suitable habitat will be cleared for this project. Species to be cleared mainly consist of boxelder (*Acer negundo*) and black willow (*Salix nigra*). Approximately 0.9 acre of permanent

ROW will be required. No permanent or temporary lighting will be required. Project work is set to begin in the Spring of 2022.

On August 22, 2019, INDOT Greenfield District personnel performed a check of the USFWS database for the presence of endangered bat species or their hibernacula within 0.5 mile of the project area and none were found.

On July 31, 2019, Lochmueller Group performed an inspection of bridge 125 for evidence or presence of bats within the bridge and none were found.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/40.19429114921679N85.46177458395874W>



Counties: Delaware, IN

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location is outside the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

March 03, 2020

Consultation Code: 03E12000-2020-I-0962

Event Code: 03E12000-2020-E-04387

Project Name: Delaware No. 125 over York Prairie Creek Bridge Replacement Project - Des. No. 1700680

Subject: Concurrence verification letter for the 'Delaware No. 125 over York Prairie Creek Bridge Replacement Project - Des. No. 1700680' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Delaware No. 125 over York Prairie Creek Bridge Replacement Project - Des. No. 1700680** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances,

Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Delaware No. 125 over York Prairie Creek Bridge Replacement Project - Des. No. 1700680

Description

The Delaware County Board of Commissioners and the Federal Highway Administration (FHWA) intend to proceed with a project involving Bridge No. 125, which carries Nebo Road over York Prairie Creek in the Town of Yorktown, Delaware County (Des. No. 1700680). The proposed project is located along Nebo Road, approximately 0.5 mile north of CR 50 South in Yorktown, Delaware County, Indiana. The proposed project will replace the existing bridge with a single-span, composite, prestressed box beam bridge with a 60-foot span. The roadway will be raised 2.5 feet across the bridge to provide adequate hydraulics for the new structure. The new cross-section of Nebo Road over the bridge will be two 11-foot travel lanes with 2-foot gutters on both sides. Additionally, a 6-foot sidewalk with a 7-inch curb will be constructed along the east side of the bridge and along the east side of Nebo Road for a total length of 767 feet. The new out-to-out coping width will be 34-foot, 7-inches with a 22-foot clear roadway width. The existing guardrail along both sides of Nebo Road will be replaced with new standard guardrail. New riprap will be placed along the spill slopes. The approach roadway, from approximately 413 feet north and 281 feet south of the structure, will be replaced to full depth and will be gently raised to accommodate the 2.5-foot raise in profile at the bridge. Additionally, 42.5 feet south and 50 feet north of the full-depth approach replacement will have 1.5 inches of its surface milled and 1.5 inches of hot mix asphalt overlay applied. The length of channel work along York Prairie Creek will be approximately 96.5 feet. The total project length along Nebo Road is 850 feet. The maintenance of traffic (MOT) plan will include the closure of Nebo Road within the project area. A detour utilizing Jackson Street, Morrison Road, and W River Road will be established. Signs, barrels, and flashing signals will be utilized along Nebo Road to notify motorists of the road closure. Access will be maintained for property owners during construction of the project. The MOT will be implemented per the current Indiana Design Manual guidelines. Suitable summer bat habitat exists within and near the project area. Approximately 0.34 acre of suitable habitat will be cleared for this project. Species to be cleared mainly consist of boxelder (*Acer negundo*) and black willow (*Salix nigra*). Approximately 0.9 acre of permanent ROW will be required. No permanent or temporary lighting will be required. Project work is set to begin in the Spring of 2022.

On August 22, 2019, INDOT Greenfield District personnel performed a check of the USFWS database for the presence of endangered bat species or their hibernacula within 0.5 mile of the project area and none were found.

On July 31, 2019, Lochmueller Group performed an inspection of bridge 125 for evidence or presence of bats within the bridge and none were found.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will the tree removal alter *any* **documented** Indiana bat or NLEB roosts and/or alter any surrounding summer habitat **within** 0.25 mile of a documented roost?

No

20. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

21. Are *all* trees that are being removed clearly demarcated?

Yes

22. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

No

23. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

24. Does the project include slash pile burning?

No

25. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

26. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

27. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *BridgeAssessmentForm_July2019.pdf* <https://ecos.fws.gov/ipac/project/POD3G7DEDBGNLDKGRVLO4CGAAM/projectDocuments/20574458>

28. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

29. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

30. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

31. Will the project involve the use of **temporary** lighting *during* the active season?

No

32. Will the project install new or replace existing **permanent** lighting?

No

33. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

34. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

35. Will the project raise the road profile **above the tree canopy**?

No

36. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

39. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

40. General AMM 1

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

41. Tree Removal AMM 1

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

42. Tree Removal AMM 3

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

43. Tree Removal AMM 4

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.34

4. Please describe the proposed bridge work:

The existing bridge will be replaced with a new bridge structure. The new structure will include sidewalk along the east side of the bridge. New riprap will be installed around each pier and at the end bents.

5. Please state the timing of all proposed bridge work:

Spring 2022

6. Please enter the date of the bridge assessment:

July 31, 2019

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 02, 2019. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

APPENDIX D: Bridge/Structure Assessment Form

This form will be completed and submitted to the District Environmental Manager by the Contractor prior to conducting any work below the deck surface either from the underside; from activities above that bore down to the underside; from activities that could impact expansion joints; from deck removal on bridges; or from structure demolition for bridges/structures within 1000 feet of suitable bat habitat.

DOT Project # 1700680	Water Body York Prairie Creek	Date/Time of Inspection 7/31/2019 10:30 AM	Within 1,000ft of suitable bat habitat (circle one) Yes No
---------------------------------	---	--	---

Route	County	Federal Structure ID
Nebo Road	Delaware	N/A; Local ID: 18-00125

If the bridge/structure is 1,000 feet or more from suitable bat habitat (e.g., an urban or agricultural area without suitable foraging habitat or corridors linking the bridge to suitable foraging habitat), check box and STOP HERE. No assessment required. ☐

Please submit to the U.S. Fish and Wildlife Service.

Areas Inspected (Check all that apply)

Bridges		Culverts/Other Structures		Summary Info (circle all that apply)			
All vertical crevices sealed at the top and 0.5-1.25" wide & ≥4" deep	X	Crevices, rough surfaces or imperfections in concrete		Human disturbance or traffic under bridge/in culvert or at the structure	High	Low	None
All crevices >12" deep & not sealed	X	Spaces between walls, ceiling joists		Possible corridors for netting	None/poor	Marginal	Excellent
All guardrails	X						
All expansion joints	X						
Spaces between concrete end walls and the bridge deck	X						

Last Revised May 31, 2017

Vertical surfaces on concrete I-beams	X						
---------------------------------------	---	--	--	--	--	--	--

Evidence of Bats (Circle all that apply) Presence of one or more indicators is sufficient evidence that bats may be using the structure.

None

Visual (e.g. survey, thermal, emergent etc.)

- Live __ number seen
- Dead __ number seen

Photo documentation Y/N

Guano

Odor Y/N

Photo documentation Y/N

Staining definitively from bats

Photo documentation Y/N

Audible

Assessment Conducted By: <u>Ruth Hook</u> Signature(s): <u><i>Ruth Hook</i></u>
District Environmental Use Only: Date Received by District Environmental Manager: _____

DOT Bat Assessment Form Instructions

1. Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges, regardless of whether assessments have been conducted in the past.
2. Any bridge/structure suspected of providing habitat for any species of bat will be removed from work schedules until such time that the DOT has coordinated with the USFWS. Additional studies may be undertaken by the DOT to determine what species may be utilizing each structure identified as supporting bats prior to allowing any work to proceed.
3. Any questions should be directed to the District Environmental Manager.

Last Revised June 2017

Chris Kunkel

From: Curry, Jennifer <JCurry1@indot.IN.gov>
Sent: Tuesday, March 3, 2020 3:44 PM
To: Kunkel, Chris
Subject: RE: Delaware 125 Bridge Project (Des. No. 1700680) IPaC Coordination

Chris,

INDOT has reviewed the determination key and has completed the verification process to forward the project to USFWS for review.

Thanks,

Jenni Curry

Environmental Manager II

Indiana Department of Transportation
32 South Broadway
Greenfield, IN 46140
317-467-3929

From: Kunkel, Chris <CKunkel@lochgroup.com>
Sent: Tuesday, March 03, 2020 3:26 PM
To: Curry, Jennifer <JCurry1@indot.IN.gov>
Cc: Kattmann, Angie <AKattmann@lochgroup.com>
Subject: RE: Delaware 125 Bridge Project (Des. No. 1700680) IPaC Coordination

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Thanks, Jenni,

I've attached Kari's email and have updated the project description.

Let me know if you have any more questions or comments.

Chris Kunkel

Environmental Biologist

Lochmueller Group

317.334.6818 (direct) | 317.677.5132 (mobile)

CKunkel@lochgroup.com

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From: Curry, Jennifer <JCurry1@indot.IN.gov>
Sent: Tuesday, March 3, 2020 2:42 PM
To: Kunkel, Chris <CKunkel@lochgroup.com>
Cc: Kattmann, Angie <AKattmann@lochgroup.com>
Subject: RE: Delaware 125 Bridge Project (Des. No. 1700680) IPaC Coordination

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-22214

Request Received: February 5, 2020

Requestor: Lochmueller Group Inc
Chris Kunkel
3502 Woodview Trace, Suite 150
Indianapolis, IN 46268

Project: Nebo Road bridge (#125) replacement over York Prairie Creek, about 0.5 mile north of CR 50 South, Yorktown; Des #1700680

County/Site info: Delaware

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal will require the formal approval of our agency for construction in a floodway pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies for a bridge exemption (see enclosure). Please include a copy of this letter with the permit application if the project does not meet the bridge exemption criteria.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Crossing Structure:

For purposes of maintaining fish and wildlife passage through a crossing structure, the Environmental Unit recommends bridges rather than culverts and bottomless culverts rather than box or pipe culverts. Wide culverts are better than narrow culverts, and culverts with shorter through lengths are better than culverts with longer through lengths. If box or pipe culverts are used, the bottoms should be buried a minimum of 6" (or 20% of the culvert height/pipe diameter, whichever is greater up to a maximum of 2') below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should: span the entire channel width (a minimum of 1.2 times the OHWM width); maintain the natural stream substrate within the structure; have a minimum openness ratio (height x width / length) of 0.25; and have stream depth, channel width, and water velocities during low-flow conditions that are approximate to those in the natural stream channel. Banklines should be restored within box and pipe structures to allow for wildlife passage above the ordinary highwater mark.

The new, replacement, or rehabbed structure, and any bank stabilization under the structure, should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. When determining an appropriate bridge or culvert size, consider whether or not wildlife/vehicle collisions are a concern at the crossing site. If feasible, a larger bridge or culvert opening can allow for the movement of wildlife under the roadway in order to minimize wildlife/vehicle collisions.

Attachments: A - Bridge Exemption Criteria

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
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2) Bank Stabilization:

Establishing vegetation along the banks is critical for stabilization and erosion control. In addition to vegetation, some other form of bank stabilization may be needed. While hard armoring alone (e.g. riprap or glacial stone) may be needed in certain instances, soft armoring and bioengineering techniques should be considered first. In many instances, one or more methods are necessary to increase the likelihood of vegetation establishment. Combining vegetation with most bank stabilization methods can provide additional bank protection and help reduce impacts upon fish and wildlife. Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Also, the following is a USDA/NRCS document that outlines many different bioengineering techniques for streambank stabilization: <http://directives.sc.egov.usda.gov/17553.wba>.

Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Riprap may be used only at the toe of the sideslopes up to the ordinary high water mark (OHWM). The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Eastern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion.

3) Riparian Habitat:

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Floodway Habitat Mitigation guidelines (and plant lists) can be found online at: <http://www.in.gov/legislative/iac/20190130-IR-312190041NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees).

4) Wetland Habitat:

Due to the presence or potential presence of wetland habitat on site, we recommend contacting and coordinating with the Indiana Department of Environmental Management (IDEM) 401 program and also the US Army Corps of Engineers (USACE) 404 program. Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT/IDNR/USFWS Memorandum of Understanding.

5) Nesting Birds/Roosting Bats:

Repairs to the bridge could affect nesting birds or roosting bats. Cliff and Barn Swallows, among other species, often nest on the underside of road bridges and many bat species roost in expansion joints and other concrete crevices on road bridges. Survey the bridges for any bird nests prior to construction. Nest surveys should occur between May 7 and September 7, which denotes the main nesting season for most bird species. If nests are found with eggs, chicks, or parents actively attending to the nest (building the nest and visiting often), then repairs should be put on hold until the nests complete their nesting cycle (to fledging) or fail (by natural causes).

The Division of Fish and Wildlife (DFW) recommends bridge maintenance activities be restricted to the period between November 1 and March 1 to avoid the summer roosting period for most bats in the central part of the State. However, some endangered bats

Attachments: A - Bridge Exemption Criteria

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

could use a bridge to roost between November and March. No matter when work is proposed, the bridge must be inspected for the presence of bats. If there is no evidence of active bat use, work can proceed. If there is evidence of active bat use, work must not occur until either the bats leave the structure for the season or a separate permit is issued to remove the bats. Please contact Linnea Petercheff (lpetercheff@dnr.in.gov) regarding permits to handle bats. If bats are present, a more formal survey to determine what species are present may be required.

The DFW recommends consulting with the State Mammologist or the US Fish and Wildlife Service before scheduling a bridge maintenance, repair, or replacement project where evidence of bat use of the structure has been observed. Information about bat use of transportation structures as well as avoidance and exclusion measures can be found at <https://www.batcon.org/pdfs/bridges/BatsBridges2.pdf> and <https://www.whitenosesyndrome.org/mmedia-education/acceptable-management-practices-for-bat-species-inhabiting-transportation-infrastructure>.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that will not be mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in regularly mowed areas only.
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
6. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds.
7. Operate equipment used to replace the bridge from the existing roadway.
8. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
9. Do not use broken concrete as riprap.
10. Underlay the riprap with a bedding layer of well graded aggregate or a geotextile to prevent piping of soil underneath the riprap.
11. Minimize the movement of resuspended bottom sediment from the immediate project area.
12. Do not deposit or allow demolition/construction materials or debris to fall or otherwise enter the waterway.
13. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
14. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
15. Do not excavate or place fill in any riparian wetland.

Attachments: A - Bridge Exemption Criteria

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Date: March 9, 2020

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Attachments: A - Bridge Exemption Criteria

Categorical Exclusion

Appendix D

**Section 106 of the National Historic
Preservation Act (NHPA)**

Minor Projects PA Project Assessment Form – Category B Projects with Archaeology Work

Date: 2/25/2020

Project Designation Number: 1700680

Route Number: Nebo Road

Project Description: Bridge Replacement, Nebo Road over York Prairie Creek, N of River Rd

The proposed project will replace Delaware County Bridge No. 125 (Bridge No. 18-00125) that carries Nebo Road over York Prairie Creek. Bridge No. 125 was built in 1954 and reconstructed in 1975. The bridge consists of three spans that total 67-feet in length with 28-feet of approach slab at either end of the structure. The out-and-out deck width of the bridge is 34.6-feet.

The proposed project will replace the existing bridge with a single-span, composite, prestressed box beam bridge with a 60-foot span. The roadway will be raised 2.5 feet across the bridge to provide adequate hydraulics for the new structure. The new cross-section of Nebo Road over the bridge will be two 11-foot travel lanes with 2-foot gutters on both sides. Additionally, a 6-foot sidewalk with a 7-inch curb will be constructed along the east side of the bridge and along the east side of Nebo Road for a total length of 767. The new out-to-out coping width will be 34-foot, 7-inches with a 22-foot clear roadway width. The existing guardrail along both sides of Nebo Road will be replaced with new standard guardrail. New riprap will be placed along the spill slopes. The approach roadway, from approximately 413-feet north and 281-feet south of the structure, will be replaced to full depth and will be gently raised to accommodate the 2.5-foot raise in profile at the bridge. Additionally, 42.5-feet south and 50-feet north of the full-depth approach replacement will have 1.5-inches of its surface milled and 1.5-inches of hot mix asphalt overlay applied. The length of channel work along York Prairie Creek will be approximately 96.5-feet. The total project length along Nebo Road is 850-feet.

Existing ROW along Nebo Road varies from 31 to 40 feet from the centerline on both sides of the roadway of Nebo Road. The proposed project will require the acquisition of approximately 0.9 acre of permanent ROW. The proposed ROW will widen to a typical width of 50 feet wide to the west and 55 feet wide to the east of the Nebo Road centerline. The anticipated maximum ROW width would be approximately 75 feet to the west and 80 feet to the east of the Nebo Road centerline. Minor tree clearing will be required for construction access and grading.

The maintenance of traffic (MOT) involves the temporary closure of Nebo Road and the implementation of a detour that utilizes River Road, Morrison Road, and Jackson Street

Feature crossed (if applicable): York Prairie Creek

Township: Mt. Pleasant

City/County: Muncie/ Delaware County

Information reviewed (please check all that apply):

General project location map ☐ USGS map ☐ Aerial photograph ☒

Written description of project area ☐ General project area photos ☐

Last revised 9-23-08

Page 1 of 5

Previously completed archaeology reports ☒ Interim Report ☒

Previously completed historic property reports ☐

Soil survey data ☐ Bridge inspection information ☒

Other (please specify): Bridge Inspection Application System (BIAS); Indiana Historic Bridge Inventory; Indiana State Historic Architectural and Archaeological Research Database (SHAARD); Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM) website; *Delaware County Interim Report*; Arc Map GIS; Delaware County GIS (accessed via <https://beacon.schneidercorp.com>); online street-view imagery; MPPA application (including maps and photographs) sent by Lochmueller Group dated February 4th, 2020 and on file at INDOT-CRO.

Curran, Michael J. and Andrew V. Martin

2019 A Phase Ia Archaeological Field Reconnaissance for a Proposed Bridge Replacement along Nebo Road over York Prairie Creek in Delaware County, Indiana (INDOT Des. No. 1700680). Report on file, INDOT, Cultural Resources Office, Indianapolis, In.

Results of the Records Review for Above-Ground Resources:

With regard to above-ground resources, an INDOT Cultural Resources historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Delaware County. No listed resources are located within 0.25 mile of the project area, a distance that serves as an adequate area of potential effects.

The Indiana Historic Sites and Structures Inventory (IHSSI) and National Register information for Delaware County are available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). The *Delaware County Interim Report* (1985; Mt. Pleasant Township) of the Indiana Historic Sites and Structures Inventory (IHSSI) was also consulted. No IHSSI documented resources are located within 0.25 mile of the project area.

According to the IHSSI rating system, generally properties rated "contributing" do not possess the level of historical or architectural significance necessary to be considered individually National Register-eligible, although they would contribute to a historic district. If they retain material integrity, properties rated "notable" might possess the necessary level of significance after further research. Properties rated "outstanding" usually possess the necessary level of significance to be considered National Register eligible, if they retain material integrity.

The INDOT-CRO historian reviewed structures adjacent to the project area utilizing online aerial, street-view photography, and the Delaware County GIS website. The project area is located in a suburban setting; the adjacent building stock ranged from mid to late-twentieth century residential buildings. None of the structures appear to possess the historic significance or material integrity required to be considered NRHP-eligible.

The most-recent inspection reports (J. Lesh; 11/27/2019), referenced via the Bridge Inspection Application System (BIAS), was consulted to review the bridge. The subject structure (Bridge No. 18-00125 /NBI No. 1800105) carries Nebo Road over York Prairie Creek and is a 3-span continuous concrete slab bridge. The bridge was built in 1959 and reconstructed in 1975. The Indiana Historic Bridge Inventory (M & H Architecture, Inc., 2009) lists the bridge as "Non Historic" (Vol. 2; Section 2, pg.410); therefore, the bridge is not eligible for inclusion in the National Register of Historic Places.

Based on the available information, as summarized above, no above-ground concerns exist.

Archaeology Report Author/Date:

Michael J. Curran and Andrew V. Martin/November 22, 2019

Summary of Archaeology Investigation Results:

An archaeological records check and Phase Ia reconnaissance survey of the project area were conducted by CRA (Curran and Martin 2019). The records check found that one previous survey had covered the eastern portion of the project area, but that no previously recorded sites have been identified within or adjacent to the project area. A 4.6 acre survey area was examined through a combination of systematic shovel probing and visual inspection of disturbed areas. Thirty-three shovel probes were placed in areas with 0% visibility. The existing r/w and the western side of the project area was found to be mostly disturbed. No archaeological sites were identified and no further work was recommended. The report was reviewed by INDOT Cultural Resources personnel who meet the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61. It is our opinion that the report is acceptable, and we concur with the evaluations and recommendations made by Curran and Martin (November 22, 2019). Therefore, there are no archaeological concerns.

Does the project appear to fall under the Minor Projects PA? yes ☒ no ☐

If yes, please specify category and number (applicable conditions are highlighted):

B-1. Replacement, repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking, under the following conditions [***BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied***]:

Condition A (Archaeological Resources)

One of the two conditions listed below must be satisfied (***EITHER Condition i or Condition ii must be satisfied***):

- i. Work occurs in previously disturbed soils; *OR*
- ii. **Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area.** If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the Division of Historic Preservation and Archaeology (DHPA) and any archaeological site form information will be entered directly into the State Historic Architectural and Archaeological Database (SHAARD) by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

One of the two conditions listed below must be satisfied (***EITHER Condition i or Condition ii must be satisfied***):

- i. **Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource;** *OR*
- ii. Work occurs adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource under one of the two additional conditions listed below

(EITHER Condition a OR Condition b must be met and field work and documentation must be completed as described below):

- a. No unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *OR*
- b. Unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible individual above-ground resource or district and ANY ONE of the conditions (1, 2, or 3) listed below must be fulfilled:
 1. Unusual features described above will not be impacted by the project. Firm commitments regarding the avoidance of these features must be listed in the MPPA determination form and the NEPA document and must be entered into the INDOT Project Commitments Database. These projects will also be flagged for quality assurance reviews by INDOT Cultural Resources Office during/after project construction.
 2. Unusual features described above have been determined not to contribute to the significance of the historic resource by INDOT Cultural Resources Office in consultation with the SHPO based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.
 3. Impacts to unusual features described above have been determined by INDOT Cultural Resources Office to be so minimal that they do not diminish any of the characteristics that contribute to the significance of the historic resource, based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.

B-12. Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed), under the following conditions *[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]*:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met *(EITHER Condition i or Condition ii must be satisfied)*:

- i. Work occurs in previously disturbed soils; *OR*
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

The conditions listed below must be met *(BOTH Condition i and Condition ii must be satisfied)*

- i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *AND*
- ii. With regard to the subject bridge, at least one of the conditions listed below is satisfied *(AT LEAST one of the conditions a, b or c, must be fulfilled)*:

- a. The latest Historic Bridge Inventory identified the bridge as non-historic (see <http://www.in.gov/indot/2531.htm>);
- b. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect AND the considerations listed in Section IV of the Program Comment do not apply;
- c. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.

If no, please explain:

Additional comments: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction in the immediate area of the find will be stopped, and the INDOT Cultural Resources Section and the Division of Historic Preservation and Archaeology will be notified immediately.

INDOT Cultural Resources staff reviewer(s): Clint Kelly and Shaun Miller

***Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.

Categorical Exclusion

Appendix E

**Red Flag Investigation
& Hazardous Materials**



Date: February 18, 2020

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division
Indiana Department of Transportation
100 N Senate Avenue, Room N642
Indianapolis, IN 46204

From: Chris Kunkel
3502 Woodview Trace, Suite 150
Indianapolis, IN 46268
ckunkel@lochgroup.com

Re: RED FLAG INVESTIGATION
Des. No. 1700680, Local Project
Bridge Replacement
Nebo Road, Bridge #125 over York Prairie Creek, North of River Road
Delaware County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: The proposed project will involve the replacement of the existing Bridge #125 (18-00125) that carries Nebo Road over York Prairie Creek, north of River Road, in the Town of Yorktown, Delaware County, Indiana. The existing continuous concrete slab bridge will be replaced with a new bridge. This project will likely involve the replacement of all the bridge components including the bridge piers and end bents. The new structure will be a single span, concrete box beam bridge with a 60-foot span. The clear roadway width will be 26 feet. The approach roadway, from approximately 413 feet north and 281 feet south of the structure, will be replaced to full depth and will be gently raised to accommodate the 2.5-foot raise in profile at the bridge. Additionally, 42.5 feet south and 50 feet north of the full-depth approach replacement will have 1.5 inches of its surface milled and 1.5 inches of hot mix asphalt overlay applied. The existing guardrail will also be replaced with new standard guardrail. The project will also involve the construction of a 6-foot wide sidewalk along the east side of Nebo Road through the project area. New riprap will be placed along the spill slopes for erosion control.

Bridge and/or Culvert Project: Yes ☒ No ☐ Structure # 18-00125

If this is a bridge project, is the bridge Historical? Yes ☐ No ☒ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Proposed right of way: Temporary ☐ # Acres N/A Permanent ☒ # Acres 0.9 , Not Applicable ☐

Type of excavation: Excavation will be required for the removal of the bridge piers, the placement of new bents, and for the placement of riprap along both sides of the stream. Excavation will not likely exceed 15 feet.

Maintenance of traffic: The maintenance of traffic (MOT) will likely involve the closure of Nebo Road within the project area. A detour will be established.

Work in waterway: Yes ☒ No ☐ Below ordinary high water mark: Yes ☒ No ☐

www.in.gov/dot/

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State Project: ☐ LPA: ☒

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	1*	Recreational Facilities	2
Airports ¹	N/A	Pipelines	1
Cemeteries	1	Railroads	1
Hospitals	N/A	Trails	1
Schools	1*	Managed Lands	N/A

¹In order to complete the required airport review, a review of public airports within 3.8 miles (20,000 feet) is required.

Explanation:

**Religious Facilities:* Although not mapped on the GIS layer, one (1) religious facility is located within the 0.5 mile search radius. The facility, Grace Baptist Church, is located 0.3 mile southeast of the project area. No impact is expected.

Cemeteries: One (1) cemetery, mapped as two GIS points, is located within the 0.5 mile search radius. The cemetery, McKinley Cemetery, is located 0.47 mile northwest of the project area. No impact is expected.

**Schools:* Although not mapped on the GIS layer, one (1) school is located within the 0.5 mile search radius. The school, Heritage Hall Christian School, is located 0.45 mile southeast of the project area. No impact is expected.

Recreational Facilities: Two (2) recreational facilities are located within the 0.5 mile search radius. The nearest facility, The Players Club Golf Course, is located adjacent to the project area. Coordination with The Players Club will occur.

Pipelines: One (1) pipeline is located within the 0.5 mile search radius. The pipeline, a natural gas pipeline owned by Indiana Gas, is 0.22 mile north of the project area. No impact is expected.

Railroads: One (1) railroad is located within the 0.5 mile search radius. The railroad is 0.45 mile northeast of the project area. No impact is expected.

Trails: One (1) trail is located within the 0.5 mile search radius. The trail, River Road – Nebo Road to CR 300 W, is located 0.42 mile south of the project area. No impact is expected.

WATER RESOURCES TABLE AND SUMMARY

Water Resources Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	9
Canal Structures – Historic	N/A	Lakes	5
NPS NRI Listed	N/A	Floodplain - DFIRM	13
NWI-Lines	4	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	2	Sinkhole Areas	N/A
Rivers and Streams	2	Sinking-Stream Basins	N/A

Explanation:

NWI-Lines: Four (4) NWI line segments are located within the 0.5 mile search radius. The nearest NWI line, York Prairie Creek, is located within the project area. A Waters of the US Report is recommended and coordination with the appropriate agency, if applicable, will occur.

IDEM 303d Listed Streams and Lakes (Impaired): Two (2) 303d Listed Stream segments are located within the 0.5 mile search radius. York Prairie Creek is located within the project area and is listed as impaired for *E. coli*. Workers who are working in or near water with *E. coli* should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure.

Rivers and Streams: Two (2) river and stream segments are located within the 0.5 mile search radius. York Prairie Creek is located within the project area. A Waters of the US Report is recommended and coordination with the appropriate agency, if applicable, will occur.

NWI-Wetlands: Nine (9) NWI wetland polygons are located within the 0.5 mile search radius. The nearest NWI wetland is 0.03 mile west of the project area. No impact is expected.

Lakes: Five (5) lakes are located within the 0.5 mile search radius. The nearest lake is 0.13 mile south of the project area. No impact is expected.

Floodplain-DFIRM: Thirteen (13) floodplain polygons are located within the 0.5 mile search radius. The project is located within five (5) floodplain polygons. Coordination with the appropriate agency will occur.

URBANIZED AREA BOUNDARY SUMMARY

Explanation: This project lies within the Town of Yorktown UAB. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. An early coordination letter with topographic and aerial maps showing the project area should be sent to the Muncie, Yorktown, Ivy Tech MS4 Coordinator at 5150 W Kilgore Ave, Muncie, Indiana 47305.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	N/A	Mineral Resources	N/A
Mines – Surface	N/A	Mines – Underground	N/A

Explanation: No Mining or Mineral Exploration Concerns are within the 0.5 mile search radius.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	N/A	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	N/A
Solid Waste Landfill	1	NPDES Facilities	N/A
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	N/A
Leaking Underground Storage (LUST) Sites	1	Notice of Contamination Sites	N/A

Explanation:

LUST: One (1) LUST site is located within the 0.5 mile search radius. The LUST site, Grace Baptist Church (Agency Interest ID: 1909), is 0.39 mile south of the project area. According to Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC), IDEM issued a No Further Action letter on September 2, 1999. No impact is expected.

Landfill Boundaries: Two (2) Landfill Boundaries are located within the 0.5 mile search radius. The nearest landfill boundary, Yorktown Dump (ID: 1248), is located 0.43 mile south of the project area. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Delaware County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is attached with ETR species highlighted. A preliminary review of the Indiana Natural Heritage Database by INDOT Environmental Services did not indicate the presence of ETR species within the 0.5 mile search radius. Coordination with IDNR and USFWS will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The project area is located in a suburban area with surrounding agricultural, recreational, and residential land. The November 19, 2018, inspection report for Bridge #125 (18-00125) states that no evidence of bats was seen or heard under the bridge. The range-wide programmatic consultation for the Indiana bat and northern long-eared bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE:

Recreational Facilities: The nearest recreational facility, The Players Club Golf Course, is located adjacent to the project area. Coordination with The Players Club will occur.

WATER RESOURCES: The presence of the following water resources will require the preparation of a Waters of the US Report and coordination with the appropriate agency, if applicable:

- One (1) NWI line is located within the project area. York Prairie Creek is located within the project area.
- York Prairie Creek is located within the project area.
- The project is located within five (5) floodplain polygons (coordination only).

York Prairie Creek is located within the project area and is listed as impaired for *E. coli*. Workers who are working in or near water with *E. coli* should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure.

URBANIZED AREA BOUNDARY: This project lies within the Town of Yorktown UAB. Post construction Storm Water Quality BMPs may need to be considered. An early coordination letter with topographic and aerial maps showing the project area should be sent to the Muncie, Yorktown, Ivy Tech MS4 Coordinator at 5150 W Kilgore Ave, Muncie, Indiana 47305.

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: N/A

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation INDOT Projects".

INDOT Environmental Services concurrence:

**Nicole Fohey
Breting**

Digitally signed by
Nicole Fohey-Breting
Date: 2020.03.05
22:13:10 -05'00'

(Signature)

Prepared by:



Chris Kunkel
Environmental Biologist
Lochmueller Group, Inc.

www.in.gov/dot/
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Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

INFRASTRUCTURE: YES

WATER RESOURCES: YES

URBANIZED AREA BOUNDARY: YES

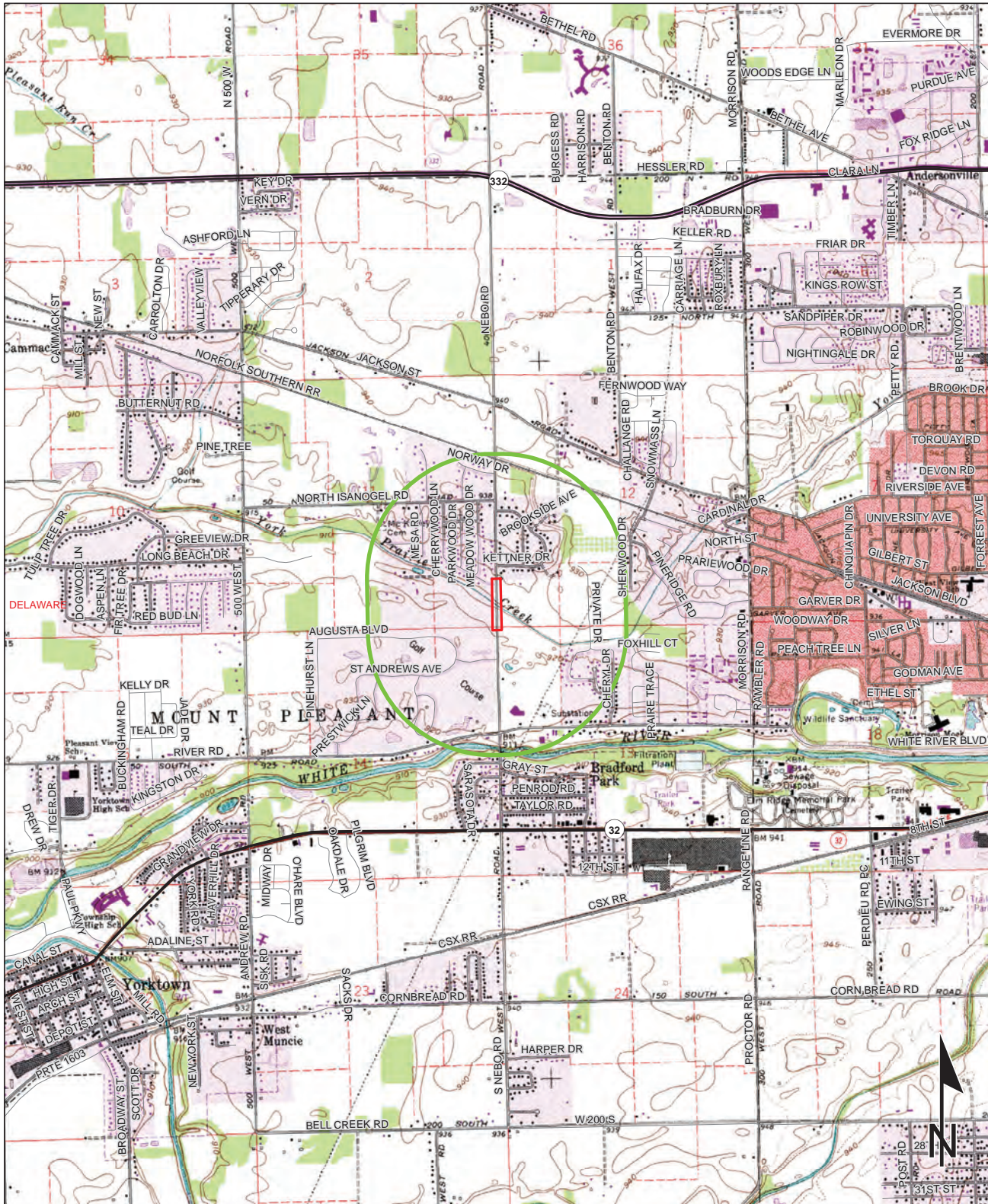
MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: YES

Additional Attachments:

Delaware County ETR List

Red Flag Investigation - Site Location
Nebo Road, North of River Road
Des. No. 1700680, Bridge Replacement
Delaware County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

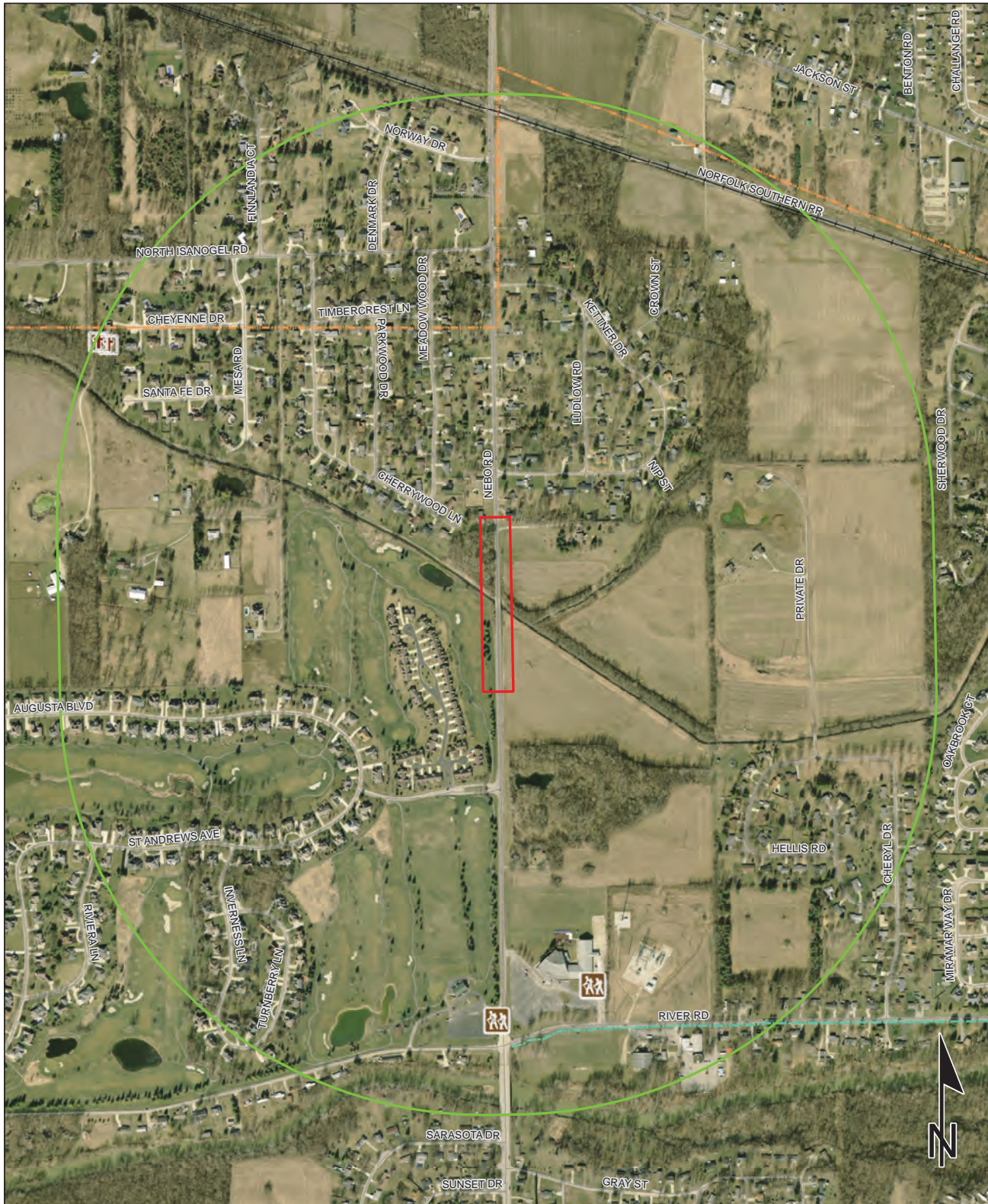
**MUNCIE WEST QUADRANGLE
INDIANA
7.5 MINUTE SERIES
(TOPOGRAPHIC)**

Red Flag Investigation - Infrastructure

Nebo Road, North of River Road

Des. No. 1700680, Bridge Replacement

Delaware County, Indiana



Sources: 0.1 0.05 0 0.1 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

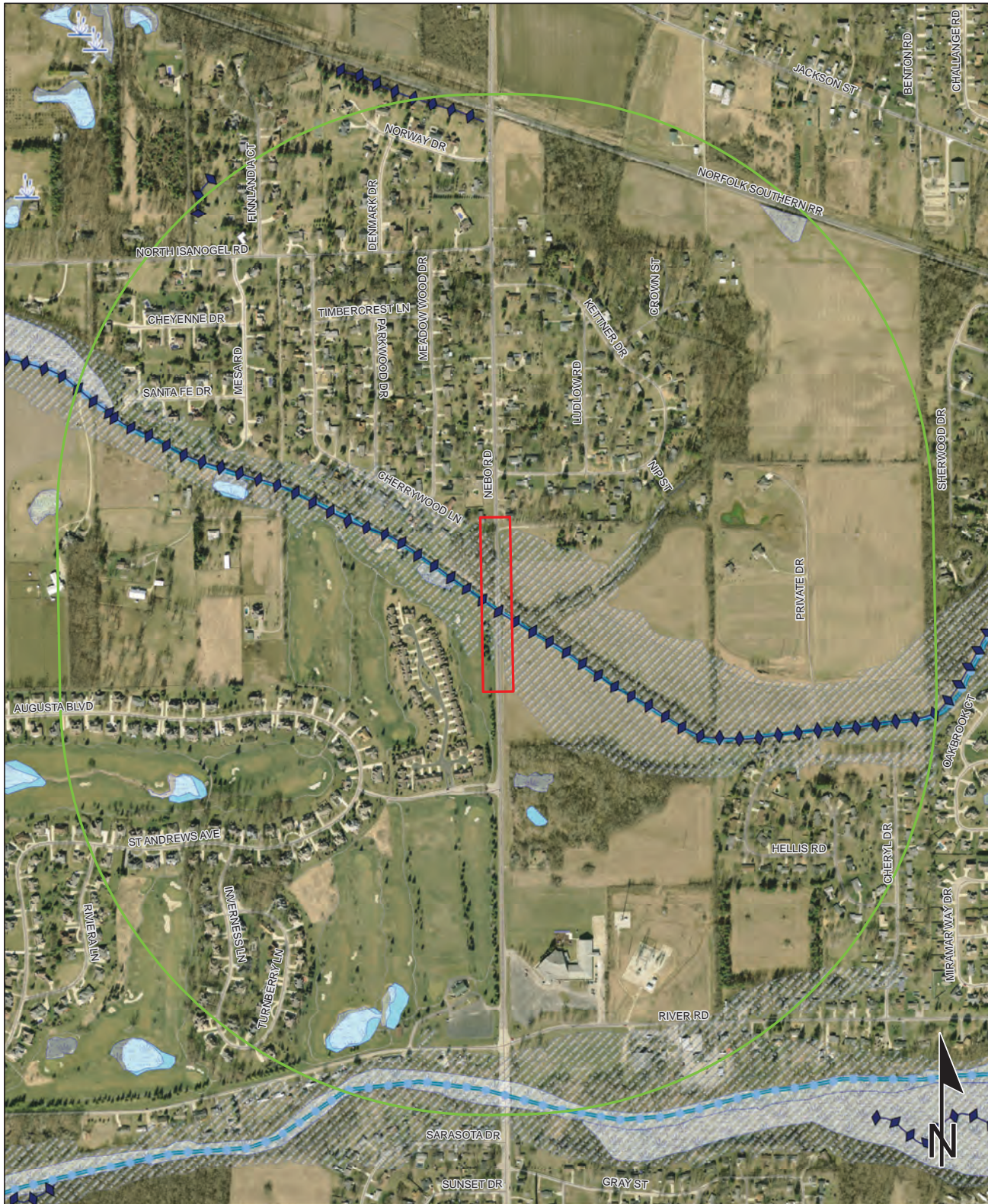
	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

Red Flag Investigation - Water Resources

Nebo Road, North of River Road

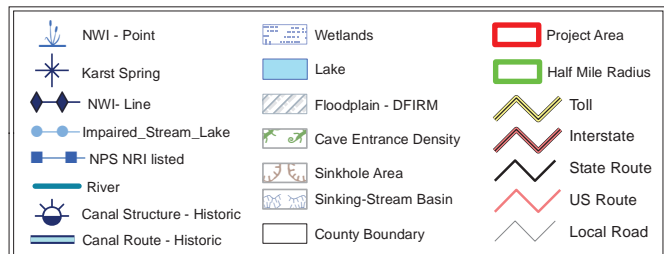
Des. No. 1700680, Bridge Replacement

Delaware County, Indiana



Sources:
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Red Flag Investigation - Urbanized Area Boundary

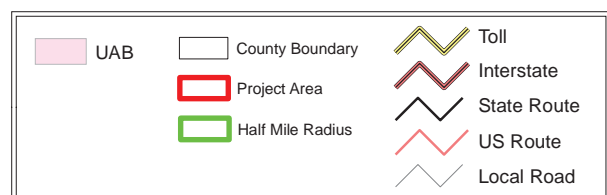
Nebo Road, North of River Road

Des. No. 1700680, Bridge Replacement

Delaware County, Indiana



Sources: 0.15 0.075 0 0.15 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

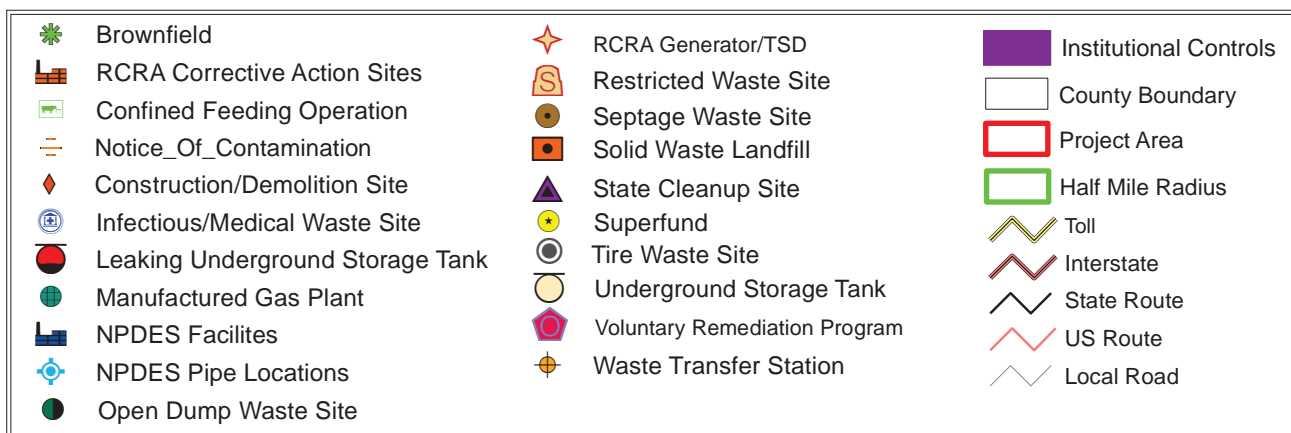


Red Flag Investigation - Hazardous Material Concerns

Nebo Road, North of River Road

Des. No. 1700680, Bridge Replacement

Delaware County, Indiana



0.15 0.075 0 0.15 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted

Des. No. 1700680 or other purposes.

Appendix E: Red Flag Investigation & Hazardous Materials Section: UTM Zone 16 N

Sources:

Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data

(www.indianamap.org)

Map Datum: NAD83

Indiana County Endangered, Threatened and Rare Species List

County: Delaware

Species Name	Common Name	FED	STATE	GRANK	SRANK
Mollusk: Bivalvia (Mussels)					
ÄÄ !"#%&\$('\$)* \$)\$	Northern Riffleshell	LE	SE	G2	S1
+\$&Ä% # %', \$%- !#\$	Wavyrayed Lampmussel		SSC	G5	S3
.#/0 (!"/ &\$'- #1\$	Clubshell	LE	SE	G1G2	S1
.#/0(!"/&\$'-!(2\$30&	Ohio Pigtoe		SSC	G4	S2
.34-5!"(\$)-50%',\$%- !#\$(%	Kidneyshell		SSC	G4G5	S2
6 0\$2(0#\$'-4#)2(-\$'-4#)2(-\$	Rabbitsfoot	LT	SE	G3G4T3	S1
7!8!#\$\$%&\$'\$ 1 20%	Purple Lilliput	C	SSC	G3Q	S2
9 ##%\$,\$"\$ \$ #? %	Rayed Bean	LE	SE	G2	S1
Insect: Hymenoptera					
: ! &"0%\$,,) %	Rusty-patched Bumble Bee	LE	SE	G1	S1
Reptile					
; #/&& 4%* 033\$3\$	Spotted Turtle	C	SE	G5	S2
; #!)!Ä5 %'< (3#\$)2	Kirtland's Snake		SE	G2	S2
Ä& 42! 2/\$""#\$)2) *	Blanding's Turtle	C	SE	G4	S2
= %3(0(0%'-3\$/\$)30%	Eastern Massasauga	LT	SE	G3	S2
75\$&!)Ä5 %""03#/(Butler's Garter Snake		SE	G4	S1
Bird					
:!3\$0(0%#/)3 *)!%0%	American Bittern		SE	G5	S2B
>\$# \$/30%#/'0-!-Ä5\$#0%	Bald Eagle		SSC	G5	S2
+\$) 0%#02! 1 - \$) 0%,	Loggerhead Shrike		SE	G4	S3B
?) !3 #3\$'1\$(\$	Black-and-white Warbler		SSC	G5	S1S2B
@4-3\$)\$%\$%'1 !#-\$-\$	Yellow-crowned Night-heron		SE	G5	S2B
@4-3 -!(\$8') 4-3 -!(\$8	Black-crowned Night-heron		SE	G5	S1B
A\$##0%'/#)\$)%%	King Rail		SE	G4	S1B
Mammal					
+\$% 0(0%""!(/\$# %	Eastern Red Bat		SSC	G3G4	S4
?4 !3 %'%/Ä3/)3(!)\$# %	Northern Long Eared Bat	LT	SE	G1G2	S2S3
?4 !3 %'%!2\$# %	Indiana Bat	LE	SE	G2	S1
./(& 4!3 %'%0#10%	Tricolored Bat		SE	G2G3	S2S3
7\$8 2/\$'3\$80%	American Badger		SSC	G5	S2
Vascular Plant					
; \$(/8'\$#!Ä/-! 2/\$	Foxtail Sedge		SE	G5	S1
; \$(/8'2/-!&Ä!% 3\$	Cypress-knee Sedge		ST	G3G4	S2
B#-/(\$""!(/\$# %	Small Floating Manna-grass		SE	G5	S1
? \$33/0-- \$'%3(035 !Ä3/(%	Ostrich Fern		SR	G5	S3
= #/)'/(* \$	Royal Catchfly		SE	G3	S1
7(-5!%3/&\$'2 -5!3!&0&	Forked Bluecurl		WL	G5	S3
7(.! #0&' %3 #) ./ (0&	Running Buffalo Clover	LE	SE	G3	S1
7(* #!-5)Ä\$#0%3(%	Marsh Arrow-grass		ST	G5	S2

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Indiana County Endangered, Threatened and Rare Species List

County: Delaware

Species Name	Common Name	FED	STATE	GRANK	SRANK
9\$#/(\$)/##\$'-5/)!Ä!2 ,!# \$	Goose-foot Corn-salad		WL	G4	S3
C %Ä (\$' .03%-/) %	American Wisteria		SR	G5	S3
High Quality Natural Community					
D!(/%3'E',#\$3F!!2%'-/)3(\$# '3 ##'Ä#\$)	Central Till Plain Flatwoods		SG	G3	S2
D!(/%3'E'0Ä#\$)2'&/% -';/)3(\$# '7 ##'.#\$)	Central Till Plain Mesic Upland Forest		SG	GNR	S3

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
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SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Categorical Exclusion

Appendix F

Water Resources

Waters of the U.S. Determination Report
Delaware Co. Bridge 125 Replacement Project
0.50 mi. N of CR 50 S
Delaware County, Indiana
Des. No. 1700680



August 23, 2019

Prepared By:



3502 Woodview Trace, Suite 150
Indianapolis, IN, 46268
Ph: 317-222-3880

Prepared For:

Delaware County Engineering Department
7700 E. Jackson Street
Muncie, Indiana 47302

Waters of the U.S. Determination Report
Delaware Co. Bridge 125 Replacement
0.5 mi. N of CR 50 S
Delaware County, Indiana
Des. No. 1700680

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Attachments

General Location Map.....	A1
USGS Topographic Map	Removed to avoid duplication; see Appendix B
USGS Topographic Map (Zoomed).....	A3
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Waters of the U.S. Determination Report
Delaware Co. Bridge 125 Replacement
0.5 mi. N of CR 50 S
Delaware County, Indiana
Des. No. 1700680

Date of Waters Investigation
July 31, 2019

Location

The project is located along Nebo Road, 0.5 mile north of CR 50 S, near Yorktown, Indiana (Attachment A1).

- Mount Pleasant Township, Delaware County, Indiana
- Sections 11, 12, 13 & 14, Township 20 North, Range 9 East
- Muncie West 1:24,000 United States Geological Survey (USGS) Quadrangle (Attachment A2 and A3)

Project Description

Delaware County with oversight by the Indiana Department of Transportation (INDOT) – Greenfield District proposes to proceed with a bridge replacement project in central Delaware County, Indiana (Des. No. 1700680). The proposed project will involve the replacement of the existing structure (18-00125) that carries Nebo Road over York Prairie Creek with a new structure.

National Wetlands Inventory (NWI)

Based on the U.S. Fish and Wildlife National Wetlands Inventory (NWI) data (www.fws.gov/wetlands/Data/State-Downloads.html), one NWI wetland polygon is located within the investigation area. One riverine, lower perennial, unconsolidated bottom, permanently flooded, excavated (R2UBHx) representing York Prairie Creek is within the investigation area. In addition to this NWI feature, there are five additional NWI wetlands within a half-mile of the investigation area (Attachment A5).

- Four (4) palustrine, unconsolidated bottom, intermittently exposed, excavated (PUBGx) wetlands
- One (1) palustrine, unconsolidated bottom, intermittently exposed, diked/impounded (PUBGh) wetland

Streams

HYDROGRAPHY_HIGHRES_FLOWLINE_NHD_USGS: Streams, Rivers, Canals, Ditches, Artificial Paths, Coastlines, Connectors, and Pipelines in Watersheds of Indiana (U. S. Geological Survey, 1:24,000, Line Shapefile) and the Muncie West 1:24,000 scale USGS topographic map indicate that York Prairie Creek is a perennial blueline feature in the project area (Attachments A2 and A3).

Soils

The Soil Survey Geographic (SSURGO) database and NRCS Web Soil Survey (<https://websoilsurvey.sc.egov.usda.gov/App/HomePage.htm>) for Delaware County includes the



following mapped soil series within the Delaware Co. Bridge 125 Replacement Project (Attachments A8-A13).

Table 1: Delaware County Soils

Map Abbreviation	Soil Name	Hydric Status (Rating)
FexB2	Fox loam, 2 to 6 percent slopes, eroded	No (6)
SmsAH	Sloan silt loam, 0 to 2 percent slopes, frequently flooded	Yes (90)
SnIA	Southwest silt loam, 0 to 1 percent slopes	Yes (95)
UemB	Urban land-Fox complex, 1 to 6 percent slopes	Unranked (0)
UetB	Urban land-Glynwood complex, 2 to 6 percent slopes	Unranked (0)
UfuA	Urban land-Millgrove complex, 0 to 1 percent slopes	Unranked (45)

Hydrology

According to the Indiana Floodplain Information Portal (<http://dnrmapping.dnr.in.gov/appsphp/fdms/>) and available FEMA floodplain maps (Attachment A6), the project crosses the 100-year floodplain and floodway for York Prairie Creek with a base floodplain elevation of 915.1 feet (NAVD 88). According to the USGS StreamStats Website (<https://water.usgs.gov/osw/streamstats/indiana.html>) York Prairie Creek has a watershed with an upstream drainage area of 4.991 square miles (Attachment A7). The 12-digit Hydrologic Unit Code (HUC) for the entirety of the project area is 051202010306 which identifies the York Prairie Creek-White River Watershed. The FEMA FIRMETTE can be found on Attachment A6.

Field Reconnaissance

Lochmueller Group conducted a field review for streams and wetlands within the investigation area for the Delaware Co. Bridge 125 Replacement Project on July 31, 2019. One stream, York Prairie Creek, and no wetland features were identified within the survey area. No roadside ditches with an OHWM were observed. One roadside drainage feature was identified in the northwest quadrant but lacked an OHWM. Identified features from the field reconnaissance can be seen in Attachments A15 to A27.

Wetland Analysis

Wetland determinations were conducted in accordance with the *U.S. Army Corps of Engineers Wetland Delineation Manual* (1987) and the *Regional Supplement of the Corps of Engineers Wetland Delineation Manual: Midwest Region 2.0* (2010). The July 2019 field investigation did not result in the identification of any wetland resources.

Stream Analysis

One perennial stream feature, York Prairie Creek, was identified during the July 2019 field investigation for the Delaware Co. Bridge 125 Replacement Project.



York Prairie Creek:

York Prairie creek shows as an perennial blue-line feature and would be classified as a riverine, lower perennial, unconsolidated bottom, permanently flooded, excavated (R2UBHx) wetland based on the classifications by Cowardin *et. al* (1979). York Prairie Creek flows from southeast to northwest underneath the structure to be replaced. There was 493 feet of York Prairie Creek evaluated during the field investigation. Approximately 236 feet are within the investigation area. Approximately 6.5 mile west of the project survey area, York Prairie Creek flows into the West Fork of the White River, a traditionally navigable water (TNW).

The reach of York Prairie Creek within the survey area is a wide, shallow, excavated perennial stream feature. The streambed is predominantly silt and sand with cobble. Both banks of the stream are steeply sloped and consist of habitat consistent with an upland forest. Upstream of the structure, two field tiles outlet into York Prairie Creek. The ordinary high water mark (OHWM) width of York Prairie Creek is 18 feet with a depth of 0.67 feet. This reach of York Prairie Creek is considered to exhibit average quality because of its vegetated banks with forested riparian habitat, presence of pools and riffles, and lack of erosion.

York Prairie Creek is likely to fall under the jurisdiction of the US Army Corps of Engineers (USACE) under Section 404 of the Clean Water Act (CWA) due to a defined bed and bank and direct connection to the West Fork of the White River, a TNW.

Table 2: Stream Summary Table

Stream Name	Photos	Lat/Long	OHWM Width (feet)	OHWM Depth (feet)	USGS Blue-line? Type?	Riffles? Pools?	Substrate	Quality	Water of the U.S.?
York Prairie Creek	10 - 17	40.1943° -85.4618°	18'	0.67'	Yes; Perennial	Yes Yes	Silt/Sand	Average	Yes

Conclusions

The July 2019 field investigation for the Delaware Co. Bridge 125 Replacement Project identified one stream features within the investigation area. York Prairie Creek would be considered a Waters of the U.S. due to its connectivity to the West Fork of the White River, which is navigable within Delaware County until Smithfield, Delaware County, Indiana. No roadside ditches with an OHWM were observed. One roadside drainage feature lacking an OHWM was observed in the northwest quadrant.

Every effort should be taken to avoid and minimize the impacts to the water resources listed above. Disturbance of a wetland or stream could result in a mitigation requirement to secure the required permits for the bridge replacement project. If construction exceeds the limits of the survey review area illustrated in this document, further field investigation will be needed. This report is this office's best judgment of water resources that are likely to be under federal jurisdiction, based on the guidelines set forth by the USACE. The final determination of jurisdictional waters is ultimately the responsibility of the USACE.

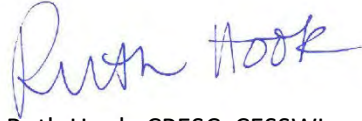


This waters determination has been prepared based on the best available information, interpreted in the light of the investigator's training, experience and professional judgement in conformance with the 1987 *Corps of Engineers Wetlands Delineation Manual*, the appropriate regional supplement, the USACE *Jurisdictional Determination Form Instructional Guidebook*, and other appropriate agency guidelines.

Preparers

Lochmueller Group, Inc. Staff	Position	Contributing Effort
Ruth Hook, CPESC, CESSWI	Environmental Biologist	Field Data Collection & Report Preparation

Signature of Preparer:

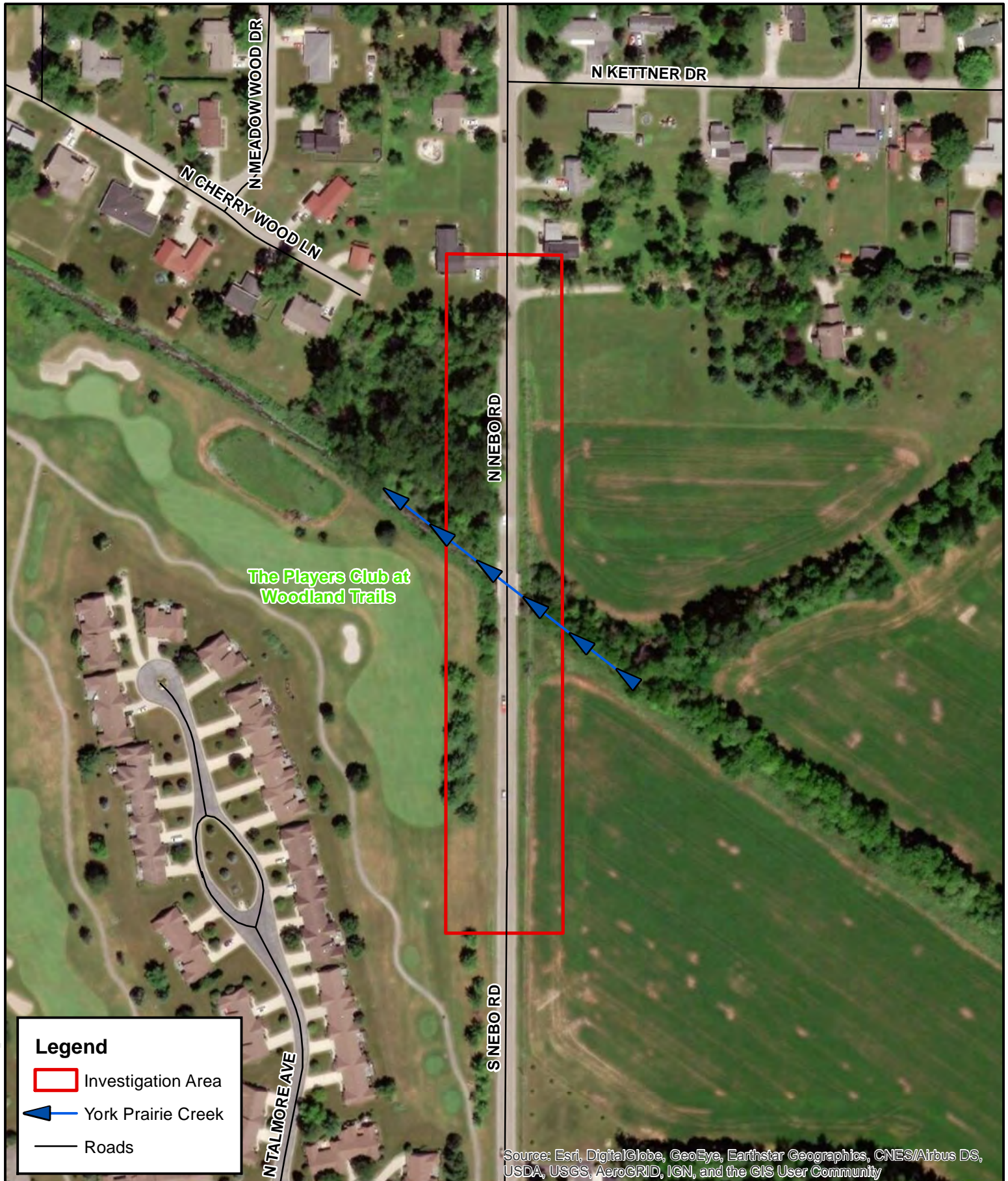


Ruth Hook, CPESC, CESSWI



ATTACHMENTS





Legend

- Investigation Area
- ↙ York Prairie Creek
- Roads

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

Water Resources Map

Des. No. 1700680
Waters of the U.S. Report

0 100 200
 Feet

County: Delaware
 Township: Mount Pleasant
 State: Indiana

Delaware Co. Bridge 125 Replacement
 0.5 mi. N of CR 50 S
 Created: 8/22/2019, R. Hook

S:\2018\219-0303\Enviro\Waters\Attachments\mxd\WaterResources_Del125.mxd



U.S. Fish and Wildlife Service

National Wetlands Inventory

Delaware Co. Bridge 125



August 19, 2019

Wetlands

Estuarine and Marine Deepwater

Estuarine and Marine Wetland

Freshwater Emergent Wetland

Freshwater Forested/Shrub Wetland

Freshwater Pond

Lake

Other

Riverine

This map is for general reference only. The US Fish and Wildlife Service is not responsible for the accuracy or currentness of the base data shown on this map. All wetlands related data should be used in accordance with the layer metadata found on the Wetlands Mapper web site.

National Flood Hazard Layer FIRMette



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
GENERAL STRUCTURES		Area of Undetermined Flood Hazard Zone D
		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		Cross Sections with 1% Annual Chance Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
		Coastal Transect Baseline
MAP PANELS		Profile Baseline
		Hydrographic Feature
		Digital Data Available
		No Digital Data Available
		Unmapped



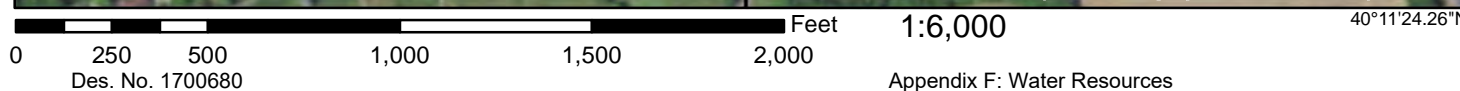
The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

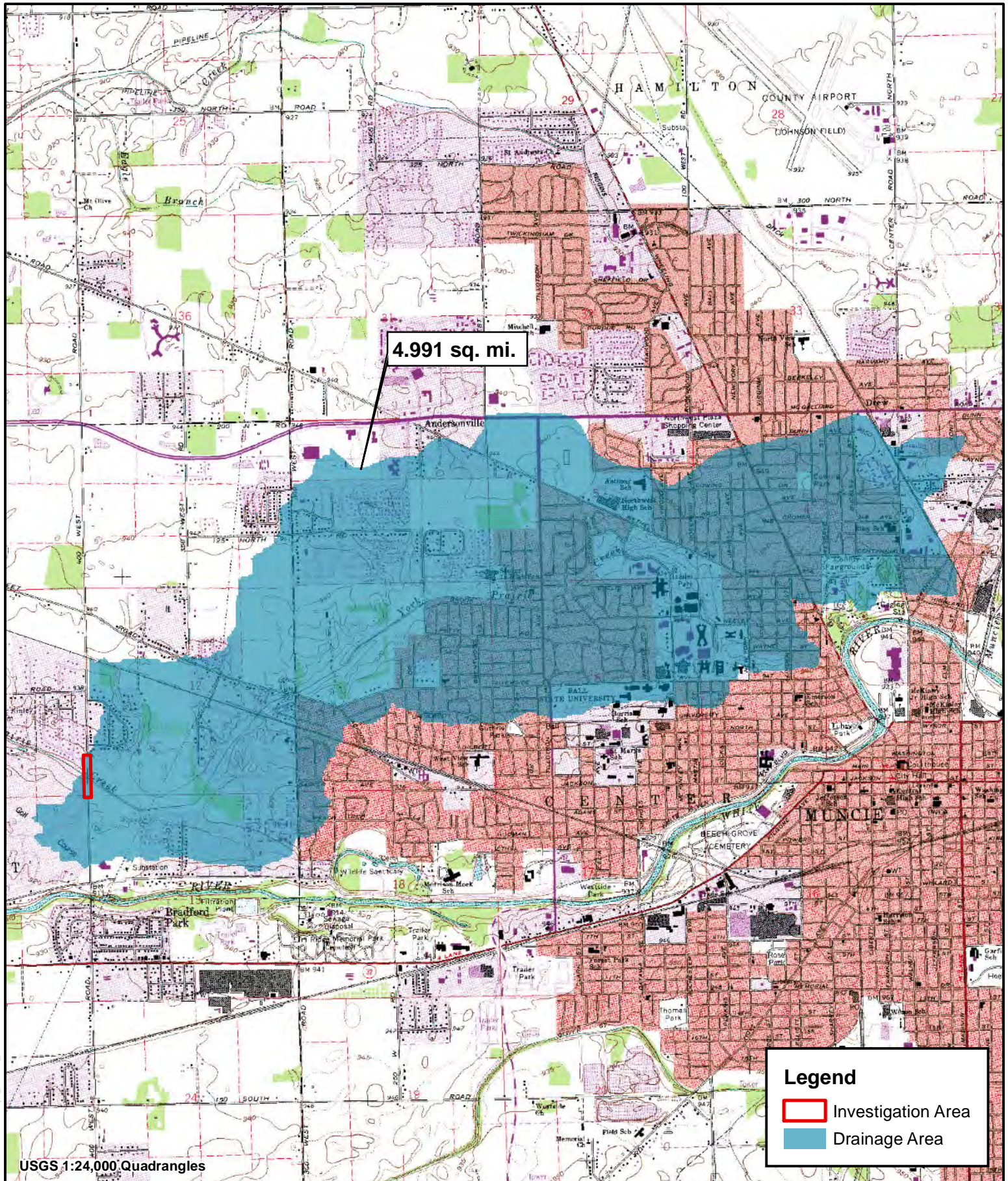
This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 8/19/2019 at 11:19:29 AM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

40°11'51.74"N





3502 Woodview Trace, Suite 150
Indianapolis, IN 46268
Phone: (317) 222-3880
Fax: (317) 222-3881

StreamStats Map

Des. No. 1700680
Waters of the U.S. Report

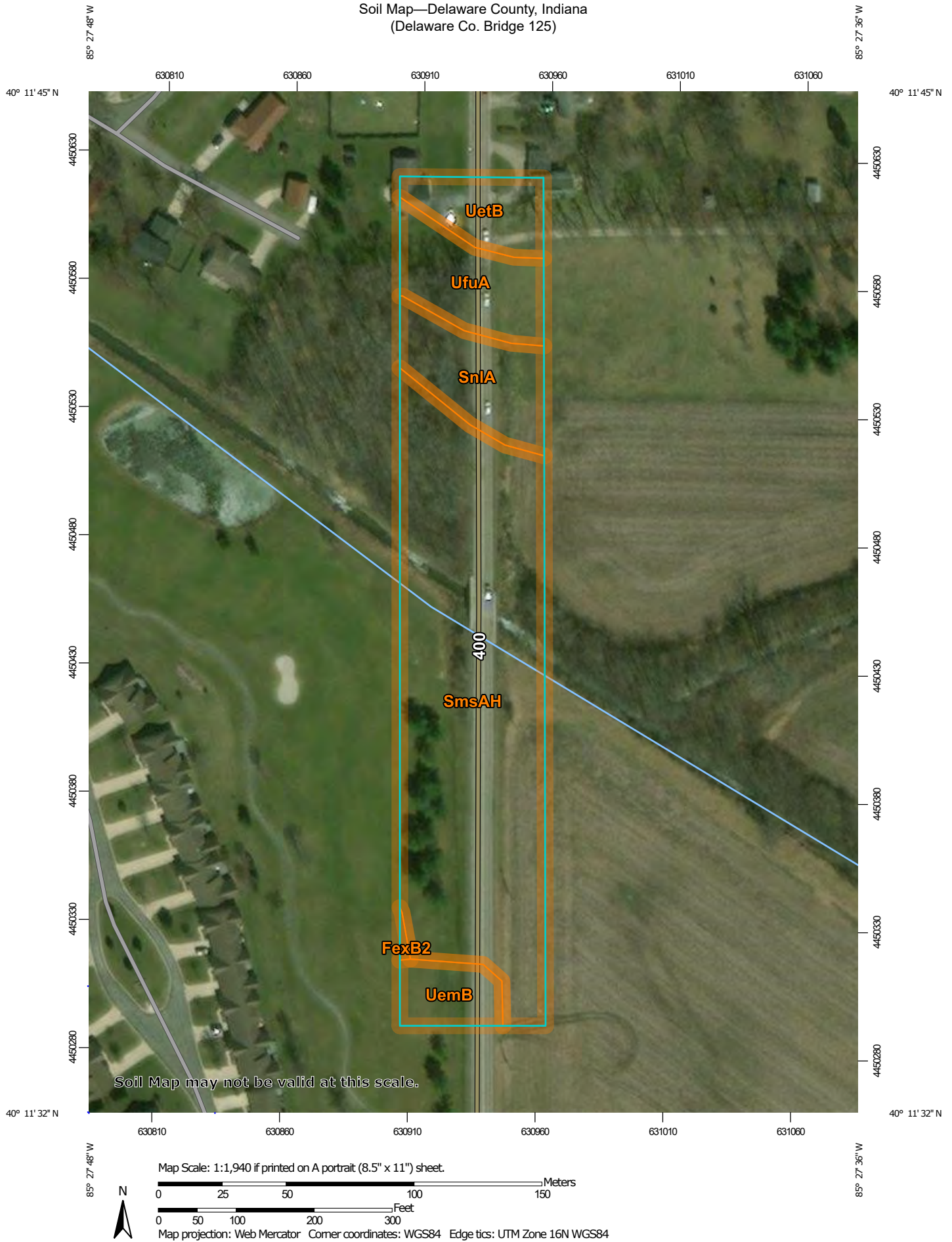
0 1,500 3,000
Feet



County: Delaware Quad: Muncie West
Township: Mount Pleasant
State: Indiana

Delaware Co. Bridge 125 Replacement
0.5 mi. N of CR 50 S
Created: 8/22/2019, R. Hook

Soil Map—Delaware County, Indiana
(Delaware Co. Bridge 125)




Natural Resources
Conservation Service

Web Soil Survey
National Cooperative Soil Survey
Appendix F: Water Resources

8/19/2019

MAP LEGEND

Area of Interest (AOI)

 Area of Interest (AOI)

Soils

 Soil Map Unit Polygons

 Soil Map Unit Lines

 Soil Map Unit Points

Special Point Features



Blowout



Borrow Pit



Clay Spot



Closed Depression



Gravel Pit



Gravelly Spot



Landfill



Lava Flow



Marsh or swamp



Mine or Quarry



Miscellaneous Water



Perennial Water



Rock Outcrop



Saline Spot



Sandy Spot



Severely Eroded Spot



Sinkhole



Slide or Slip



Sodic Spot



Spoil Area



Stony Spot



Very Stony Spot



Wet Spot



Other



Special Line Features

Water Features



Streams and Canals

Transportation



Rails



Interstate Highways



US Routes



Major Roads



Local Roads

Background



Aerial Photography

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:12,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service

Web Soil Survey URL:

Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Delaware County, Indiana

Survey Area Data: Version 23, Sep 6, 2018

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Oct 1, 2011—Apr 1, 2017

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
FexB2	Fox loam, 2 to 6 percent slopes, eroded	0.0	0.2%
SmsAH	Sloan silt loam, 0 to 2 percent slopes, frequently flooded	3.1	66.0%
SnIA	Southwest silt loam, 0 to 1 percent slopes	0.5	10.9%
UemB	Urban land-Fox complex, 1 to 6 percent slopes	0.2	5.2%
UetB	Urban land-Glynwood complex, 2 to 6 percent slopes	0.3	7.1%
UfuA	Urban land-Millgrove complex, 0 to 1 percent slopes	0.5	10.6%
Totals for Area of Interest		4.6	100.0%

Report—Hydric Soil List - All Components

Hydric Soil List - All Components--IN035-Delaware County, Indiana					
Map symbol and map unit name	Component/Local Phase	Comp. pct.	Landform	Hydric status	Hydric criteria met (code)
FexB2: Fox loam, 2 to 6 percent slopes, eroded	Fox-Eroded	70-100	Stream terraces, till plains	No	—
	Ockley	0-10	Stream terraces	No	—
	Fox-Till substratum	0-10	Stream terraces on till plains	No	—
	Westland-Drained	0-10	Swales on stream terraces, depressions on stream terraces	Yes	2
SmsAH: Sloan silt loam, 0 to 2 percent slopes, frequently flooded	Sloan	70-90	Backswamps on flood plains, flood-plain steps on flood plains, meander scars on flood plains	Yes	2
	Shoals	0-10	Flood-plain steps	No	—
	Southwest-Drained	0-10	Depressions, drainage ways	Yes	2,3
	Lash	0-10	Flood plains	No	—
	Bellcreek	0-10	Flood plains	Yes	2,3
SnIA: Southwest silt loam, 0 to 1 percent slopes	Southwest	70-90	Depressions on till plains	Yes	2,3
	Benadum	0-15	Depressions on till plains	Yes	2,3
	Sloan	0-10	Flood plains	Yes	2,3
	Shoals	0-10	Flood plains	No	—
UemB: Urban land-Fox complex, 1 to 6 percent slopes	Urban land	35-60	Terraces	Unranked	—
	Fox	25-35	Terraces	No	—
	Martinsville	0-10	Outwash plains	No	—
	Udorthents-Loamy-skeletal	0-10	Kames, outwash terraces	No	—
	Ross	0-10	Flood plains	No	—
	Udorthents-Loamy	0-10	Till plains	No	—
	Ockley	0-10	Outwash terraces	No	—
UetB: Urban land-Glynwood complex, 2 to 6 percent slopes	Urban land	35-60	Till plains	Unranked	—
	Glynwood	20-40	Till plains	No	—
	Blount	0-10	Till plains	No	—
	Rawson	0-10	Till plains	No	—

Hydric Soil List - All Components--IN035-Delaware County, Indiana					
Map symbol and map unit name	Component/Local Phase	Comp. pct.	Landform	Hydric status	Hydric criteria met (code)
	Mississinewa-Severly eroded	0-10	Till plains	No	—
	Morley	0-10	Till plains	No	—
	Udorthents-Loamy	0-10	Till plains	No	—
UfuA: Urban land-Millgrove complex, 0 to 1 percent slopes	Urban land	35-60	Outwash plains	Unranked	—
	Millgrove	20-40	Depressions on outwash plains	Yes	2,3
	Muskego	0-10	Depressions on outwash plains	Yes	1,2,3
	Milford	0-10	Glacial drainage channels	Yes	2,3
	Udorthents-Loamy	0-10	Outwash plains	No	—
	Digby	0-10	Glacial drainage channels, outwash plains	No	—
	Sloan	0-10	Flood plains	Yes	2,3

Data Source Information

Soil Survey Area: Delaware County, Indiana

Survey Area Data: Version 23, Sep 6, 2018

Hydric Rating by Map Unit

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
FexB2	Fox loam, 2 to 6 percent slopes, eroded	6	0.0	0.2%
SmsAH	Sloan silt loam, 0 to 2 percent slopes, frequently flooded	90	3.1	66.0%
SnIA	Southwest silt loam, 0 to 1 percent slopes	95	0.5	10.9%
UemB	Urban land-Fox complex, 1 to 6 percent slopes	0	0.2	5.2%
UetB	Urban land-Glynwood complex, 2 to 6 percent slopes	0	0.3	7.1%
UfuA	Urban land-Millgrove complex, 0 to 1 percent slopes	45	0.5	10.6%
Totals for Area of Interest			4.6	100.0%

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PJD: August 19, 2019

B. NAME AND ADDRESS OF PERSON REQUESTING PJD: Ruth Hook, 3502 Woodview Trace, Indianapolis, IN 46268

C. DISTRICT OFFICE, FILE NAME, AND NUMBER:

D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:

Delaware County with oversight by the Indiana Department of Transportation (INDOT) – Greenfield District proposes to proceed with a bridge replacement project in central Delaware County, Indiana (Des. No. 1700680). The proposed project will involve the replacement of the existing structure (18-00125) that carries Nebo Road over York Prairie Creek with a new structure.

(USE THE TABLE BELOW TO DOCUMENT MULTIPLE AQUATIC RESOURCES AND/OR AQUATIC RESOURCES AT DIFFERENT SITES)

State: **IN** County/parish/borough: **Delaware** City: **Yorktown**

Center coordinates of site (lat/long in degree decimal format):

Lat.: **40.1943** Long.: **-85.4618**

Universal Transverse Mercator: 630930.05 E, 4450457.40 N Z 16T

Name of nearest waterbody: **York Prairie Creek**

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

☐ Office (Desk) Determination. Date:

☐ Field Determination. Date(s):

TABLE OF AQUATIC RESOURCES IN REVIEW AREA WHICH “MAY BE” SUBJECT TO REGULATORY JURISDICTION.

Site number	Latitude (decimal degrees)	Longitude (decimal degrees)	Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable)	Type of aquatic resource (i.e., wetland vs. non-wetland waters)	Geographic authority to which the aquatic resource “may be” subject (i.e., Section 404 or Section 10/404)
York Prairie Creek	40.1943	-85.4618	236 ft (0.1 ac)	non-wetland waters	Section 404

- 1) The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.
- 2) In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring “pre-construction notification” (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant’s acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there “*may be*” waters of the U.S. and/or that there “*may be*” navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:

SUPPORTING DATA. Data reviewed for PJD (check all that apply)

Checked items should be included in subject file. Appropriately reference sources below where indicated for all checked items:

- ☒ Maps, plans, plots or plat submitted by or on behalf of the PJD requestor:
Map: Aerial, water resources, NWI, topographic, Streamstats, soils.
- ☒ Data sheets prepared/submitted by or on behalf of the PJD requestor.
☐ Office concurs with data sheets/delineation report.
☐ Office does not concur with data sheets/delineation report. Rationale: _____.
- ☐ Data sheets prepared by the Corps: _____.
- ☐ Corps navigable waters' study: _____.
- ☒ U.S. Geological Survey Hydrologic Atlas: Hydrography_HighRes_Flowline_NHD_USGS.shp.
☒ USGS NHD data.
☐ USGS 8 and 12 digit HUC maps.
- ☒ U.S. Geological Survey map(s). Cite scale & quad name: Muncie West 1:24,000 Quadrangle.
- ☒ Natural Resources Conservation Service Soil Survey. Citation: NRCS web soil survey - Del. Co..
- ☒ National wetlands inventory map(s). Cite name: USFWS NWI wetland mapper.
- ☐ State/local wetland inventory map(s): _____.
- ☒ FEMA/FIRM maps: 18085C0229E, effective date: 08/02/2017.
- ☒ 100-year Floodplain Elevation is: 915.1 feet NAVD88. (National Geodetic Vertical Datum of 1929)
- ☒ Photographs: ☒ Aerial (Name & Date): ESRI Basemap 2018.
or ☒ Other (Name & Date): Field photos from 7/31/2019.
- ☐ Previous determination(s). File no. and date of response letter: _____.
- ☐ Other information (please specify): _____.

IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.

Signature and date of
Regulatory staff member
completing PJD

Ruth Hook

Digitally signed by Ruth Hook
Date: 2019.08.19 14:20:43 -04'00'

Signature and date of
person requesting PJD
(REQUIRED, unless obtaining
the signature is impracticable)¹

¹ Districts may establish timeframes for requestor to return signed PJD forms. If the requestor does not respond within the established time frame, the district may presume concurrence and no additional follow up is necessary prior to finalizing an action.

Categorical Exclusion

Appendix G

Public Involvement



April 30, 2019

RE: Nebo Road over York Prairie Creek-Bridge 125 Replacement Project (DES #1700680)

NOTICE OF SURVEY

Dear Property Owner:

Available records indicate that you own or occupy property near or adjacent to a bridge replacement project. Lochmueller Group, Inc. has partnered Northpointe Engineering & Surveying, Inc. to perform and manage the survey of the project area. In the near future, it may be necessary for our firm or subconsultant to access your property to complete the work. This is permitted by law per Indiana Code IC 36-9-25-10(2), IC 25-21.5-9-7, and IC 8-23-7-26. If you are available, employees from our firm (or our subconsultant) will show their identification before coming onto your property.

The survey work will include mapping the location of features (i.e., sidewalks, trees, buildings, fences, utilities, and driveways) and obtaining ground elevations. The proposed survey will include locating sanitary, storm and water structures that may be located on your property. The survey is needed to depict existing conditions for the proper planning and design of the improvement project. The survey work may also include identification and mapping of wetlands.

Please rest assured that we will work to cause you as little inconvenience as possible during this survey. If you have any questions, or if any problems do occur, please contact Sean Suttles of Lochmueller Group, Inc. at the telephone number or address below.

PROJECT CONTACT

Sean Suttles, PS, CFedS
Lochmueller Group, Inc.
Phone: 812-759-4164
Email: ssuttles@lochgroup.com

SURVEY CONTACT

Jacob T. Hoffman, EI, PS
Northpointe Engineering and Surveying
Phone: 317-721-0036
Email: jhoffman@npesindy.com

Sincerely,
NORTHPOINTE ENGINEERING & SURVEYING, INC.

Jacob T. Hoffman, EI, PS
Senior Project Manager

6125 South East Street, Suite "B", Indianapolis, Indiana 46227
Office – 317.884.3020/ Fax – 317.721.0027/ www.npesindy.com
Engineering Surveying Consulting Inspection



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204-2216

Eric J. Holcomb, Governor
Joe McGuinness, Commissioner

Indiana Department of Transportation Notice of Entry for Survey or Investigation Indiana Department of Transportation

If you have received a “Notice of Entry for Survey or Investigation” from INDOT or an INDOT representative, you may be wondering what it means. In the early stages of a project’s development, INDOT must collect as much information as possible to ensure that sound decisions are made in designing the proposed project. Before entering onto private property to collect that data, INDOT is required to notify landowners that personnel will be in the area and may need to enter onto their property. Indiana Code, Title 8, Article 23, Chapter 7, Section 26 deals with the department’s authority to enter onto any property within Indiana.

Receipt of a Notice of Entry for Survey or Investigation does not necessarily mean that INDOT will be buying property from you. It doesn’t even necessarily mean that the project will involve your property at all. Since the Notice of Entry for Survey or Investigation is sent out in the very early stages and since we want to collect data within AND surrounding the project’s limits more landowners are contacted than will actually fall within the eventual project limits. It may also be that your property falls within the project limits but we will not need to purchase property from you to make improvements to the roadway. Another thing to keep in mind is that when you receive a Notice of Entry for Survey or Investigation, very few specifics have been worked out and actual construction of the project may be several years in the future.

Before INDOT begins a project that requires them to purchase property from landowners, they must first offer the opportunity for a public hearing. If you were on the list of people who received a Notice of Entry for Survey or Investigation, you should also receive a notice informing you of your opportunity to request a public hearing. These notices will also be published in your local newspaper so interested individuals who are not adjacent to the project will also have the opportunity to request a public hearing. If a public hearing is to be held, INDOT will publicize the date, location, and time. INDOT will present detailed project information at the public hearing, comments will be taken from the public in spoken and written form, and question and answer sessions will be offered. Based on the feedback INDOT receives from the public, a project can be modified and improved to better serve the public.

So, if you have received a “Notice of Entry for Survey or Investigation”, remember:

1. You do not need to take any action at this time. It is merely letting you know that people in orange/lime vests are going to be in your neighborhood.
2. The project is still in its very early planning stages.
3. You will be notified of your opportunity to comment on the project at a later date.

www.in.gov/dot/
An Equal Opportunity Employer

Categorical Exclusion

Appendix H

Air Quality

TRANSPORTATION IMPROVEMENT PROGRAM - LOCAL URBAN PROJECTS

ROJECT NUMBER	PROJECT TYPE	PROJECT DESCRIPTION	PHASE	COST	FUND CODE	State Fiscal Year	Est. Cost to Complete Totals
Des # 1173229 City of Muncie R35802	Reconstruction	Wheeling Avenue from Centennial Avenue to McGalliard Road: reconstruct to current 3 and 5 lanes.	PE " RW1 " " CN " " " CE " CE "	172,489 43,122 189,710 596,558 196,567 689,353 1,710,107 1,000,000 849,865 158,550 39,638 200,044 50,011	PYB Local Earmark STBG Local Earmark STBG PYB Local STBG Local STBG Local	Prev Prev Prev. Prev Prev 2019 2019 2019 2019 2019 2019 2020 2020	\$ 4,565,137 PE: 215,611 RW: 982,835 CN: 4,249,325 CE: 448,243 FY19: 198,188 FY20: 250,055
Des#1592998 Yorktown R39110	Pedestrian	Nebo Road, from SR 32 to Sarasota Drive: new sidewalk one side.	PE " CN/C.E. "	111,184 27,796 405,000 101,250	CMAQ Local CMAQ Local	Prev Prev 2020 2020	\$ 645,230 PE: 138,980 CN: 450,000 CE: 56,250
Des#1702868 Yorktown R39110	Pedestrian	Nebo Road, from Sarasota Drive to River Rd (BR 124): new sidewalk one side.	CN/C.E. " "	380,965 163,917 136,221	CMAQ STBG Local	2020 2020 2020	\$ 681,103 CN: 681,103
Des#1700678 Yorktown R40329	Roundabout	River Road at Nebo Road and approaches: new roundabout.	PE " RW " CN/CE "	240,000 60,000 60,000 15,000 1,215,000 303,750	CMAQ Local CMAQ Local CMAQ Local	2018 2018 2020 2020 2022 2022	\$ 1,893,750 PE: 300,000 RW: 75,000 CN: 1,518,750
Des#1700680 Delaware Co. B40331	Bridge Replacement	*BR #125, Nebo Road over York Prairie Creek, south of Kettner Drive: bridge replacement.	PE " RW " CN "	239,520 59,880 52,000 13,000 1,300,000 325,000	STBG CBR STBG CBR STBG CBR	2019 2019 2020 2020 2022 2022	\$ 1,714,400 PE: 299,400 RW: 65,000 CN: 1,625,000
Des#1700681 Delaware Co. B40332	Bridge Replacement	BR #127, CR 600W over York Prairie Creek, north of Lone-beech: bridge replacement.	CN/CE "	1,022,000 255,500	STP-U CBR	2020 2020	\$ 1,277,500 CN: 1,190,000 CE: 87,500
Des#1700682 Delaware Co. B40333	Bridge Replacement	BR #502, Gharkey St. over Buck Creek, north of 23 rd St.: bridge replacement.	PE " RW "	216,000 54,000 128,000 32,000	STBG CBR STBG CBR	2021 2021 2022 2022	\$ 2,185,000 PE: 270,000 RW: 160,000
Des#1700751 City of Muncie R40343	Bicycle Pedestrian	Cultural Trail Phase 1: bicycle/pedestrian paths routing from Dicks to E. Main Street along University, Pauline, Wysor and Madison.	PE " CN/CE "	330,000 96,600 2,737,779 684,445	STBG Local STP-TA Local	Prev Prev 2021 2021	\$ 2,390,987 PE: 426,600 CN: 3,055,557 CE: 366,667
Des#1700752 City of Muncie R40344	Road Reconstruction	Riggin Road reconstruction: from Wheeling Avenue to Old SR 3. (CN in FY 2024)	PE "	320,000 80,000	STBG Local	2023 2023	\$ 3,800,000 PE: 400,000
Des#1801325 Delaware Co.	PLANNING	FY 2020 STBG funds in the Unified Planning Work Program.	PE "	55,000 13,750	STBG Local	2020 2020	\$ 329,813 19PE: 329,813
Des#1900774 City of Muncie	Bicycle Pedestrian	Kitselman Trail Phase 3: bicycle/pedestrian trail work on west side of White River.	PE " CN/CE "	240,000 60,000 2,400,000 600,000	STP-TA Local STP-TA Local	2020 2020 2023 2023	\$ 3,300,000 PE: 300,000 CN: 3,000,000

*Project location is also listed elsewhere as north of River Road (CR 50 S).

Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2020 - 2024

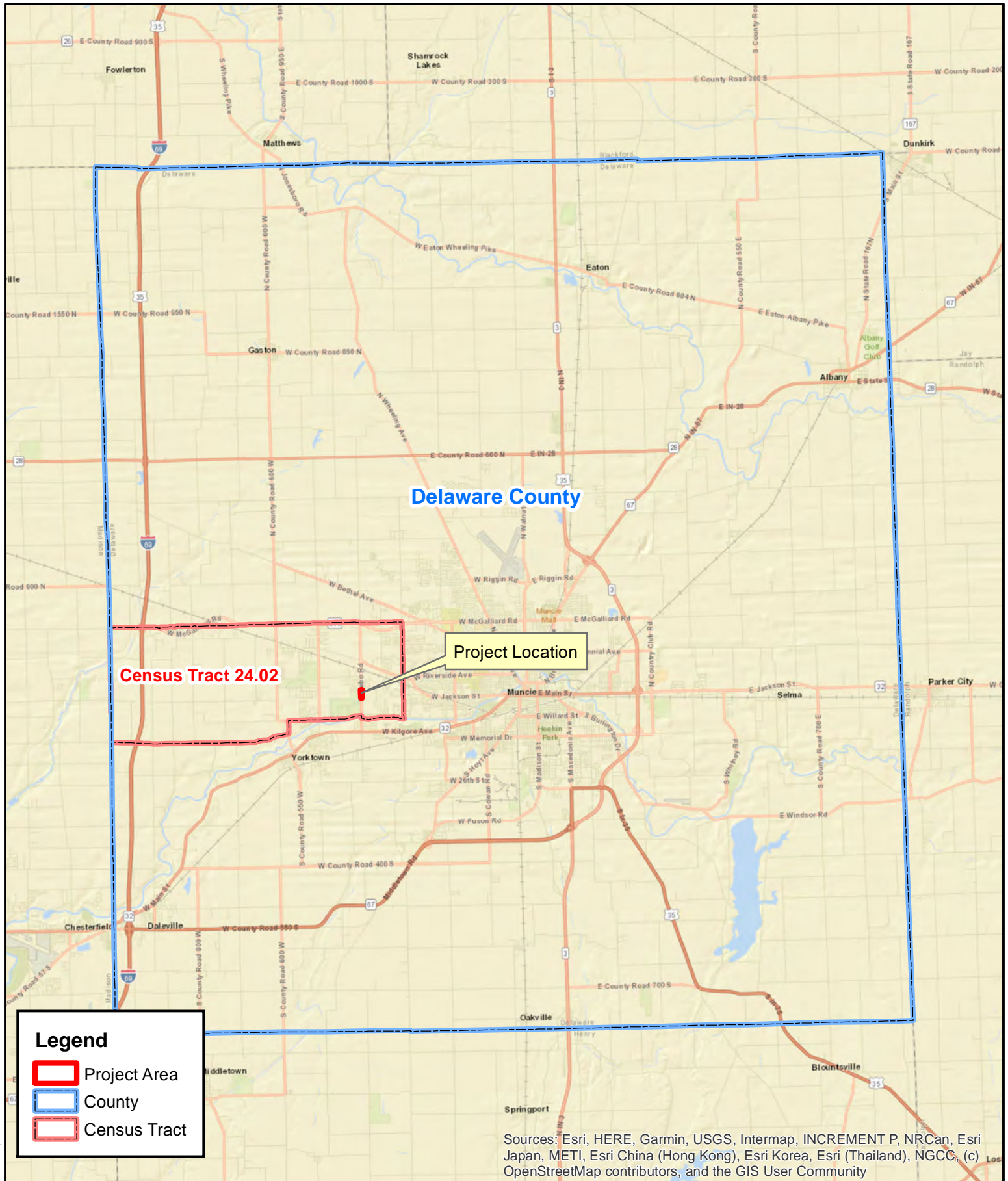
SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2020	2021	2022	2023	2024
Indiana Department of Transportation	39995 / 1593199	M 05	SR 32	HMA Overlay, Preventive Maintenance	From SR 9 N Jct to US 35	Greenfield	19.167	STBG	\$10,954,373.00	Bridge Construction	CN	\$0.00	\$0.00	(\$247,797.00)	\$247,797.00			
Comments:MPO Moving CN funds from 2020 to 2021 DMMPC 19-18																		
Indiana Department of Transportation	40009 / 1602210	Init.	I 69	Replace Superstructure	NB over Pipe Creek, 03.95 N SR 28	Greenfield	0	NHPP		Bridge Construction	CN	\$2,645,523.00	\$293,947.00	\$2,939,470.00				
Indiana Department of Transportation	40011 / 1602238	Init.	SR 332	Bridge Deck Overlay	Over I-69 NB/SB	Greenfield	0	STPBG		Bridge Construction	CN	\$1,731,452.00	\$432,863.00	\$2,164,315.00				
Yorktown	40329 / 1700678	Init.	ST 1010	Intersection Improvement, Roundabout	Intersection of Nebo Road and River Road, including approaches	Greenfield	0	STPBG		Muncie MPO	CN	\$1,080,000.00	\$0.00			\$1,080,000.00		
										Muncie MPO	RW	\$80,000.00	\$0.00	\$80,000.00				
										Local Funds	CN	\$0.00	\$270,000.00			\$270,000.00		
										Local Funds	RW	\$0.00	\$20,000.00	\$20,000.00				
Yorktown	40329 / 1700678	A 14	ST 1010	Intersection Improvement, Roundabout	Intersection of Nebo Road and River Road, including approaches	Greenfield	0	STBG	\$1,500,000.00	Muncie MPO	CN	\$135,000.00	\$0.00			\$135,000.00		
										Local Funds	CN	\$0.00	\$33,750.00			\$33,750.00		
Comments:MPO initial TIP.																		
Delaware County	40331 / 1700680	Init.	ST 1010	Bridge Replacement, Other Construction	Bridge #125 Nebo Road over York Prairie Creek, N of River Rd	Greenfield	0	STPBG		Muncie MPO	CN	\$1,080,000.00	\$0.00			\$1,080,000.00		
										Muncie MPO	RW	\$52,000.00	\$0.00	\$52,000.00				
										Local Funds	CN	\$0.00	\$270,000.00			\$270,000.00		
										Local Funds	RW	\$0.00	\$13,000.00	\$13,000.00				
Delaware County	40331 / 1700680	A 14	ST 1010	Bridge Replacement, Other Construction	Bridge #125 Nebo Road over York Prairie Creek, N of River Rd	Greenfield	0	STBG	\$1,600,000.00	Muncie MPO	CN	\$1,300,000.00	\$0.00			\$1,300,000.00		
										Local Funds	CN	\$0.00	\$325,000.00			\$325,000.00		
Comments:Initial MPO TIP.																		
Delaware County	40332 / 1700681	Init.	IR 1026	Bridge Replacement, Other Construction	Bridge #127, CR 600W over York Prairie, N of Lonebeech Dr	Greenfield	0	STPBG		Muncie MPO	CN	\$630,000.00	\$0.00	\$630,000.00				
										Local Funds	CN	\$0.00	\$157,500.00	\$157,500.00				

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

Categorical Exclusion

Appendix I

**Environmental Justice
Analysis**



LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
Indianapolis, IN 46268
Phone: (317) 222-3880
Fax: (317) 222-3881

**Environmental Justice
Analysis Map**
Des. No. 1700680



County: Delaware
Township: Mt. Pleasant
State: Indiana

Delaware 125 Bridge Project
Nebo Road, north of River Road
Created: 3/6/2020, C Kunkel

	COC Delaware County, Indiana	AC 1 Census Tract 24.02
LOW-INCOME POPULATION		
Total Population for Whom Poverty Status is Determined	108,823	8,687
Total Population Below Poverty Level	23,018	496
Percent Low-Income	21.2%	5.7%
125 Percent of COC	26.4%	
AC Percent Low-Income Greater Than 125 Percent of COC?		No
AC Percent Low-Income Greater Than 50 Percent?		No
Population of EJ Concern?		No
MINORITY POPULATION		
Total Population	115,938	8,733
Minority Population	14,982	561
Percent Minority	12.9%	6.4%
125 Percent of COC	16.2%	
AC Percent Minority Greater Than 125 Percent of COC?		No
AC Percent Minority Greater Than 50 Percent?		No
Population of EJ Concern?		No

B03002

HISPANIC OR LATINO ORIGIN BY RACE

Universe: Total population

2013-2017 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.

	Delaware County, Indiana		Census Tract 24.02, Delaware County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error
Total:	115,938	*****	8,733	+/-312
Not Hispanic or Latino:	113,277	*****	8,682	+/-323
White alone	100,956	+/-79	8,172	+/-391
Black or African American alone	8,275	+/-266	352	+/-258
American Indian and Alaska Native alone	69	+/-39	0	+/-16
Asian alone	1,563	+/-231	53	+/-81
Native Hawaiian and Other Pacific Islander alone	44	+/-36	0	+/-16
Some other race alone	166	+/-130	0	+/-16
Two or more races:	2,204	+/-352	105	+/-99
Two races including Some other race	63	+/-47	0	+/-16
Two races excluding Some other race, and three or more races	2,141	+/-337	105	+/-99
Hispanic or Latino:	2,661	*****	51	+/-52
White alone	1,613	+/-259	51	+/-52
Black or African American alone	188	+/-142	0	+/-16
American Indian and Alaska Native alone	19	+/-22	0	+/-16
Asian alone	0	+/-27	0	+/-16
Native Hawaiian and Other Pacific Islander alone	71	+/-105	0	+/-16
Some other race alone	459	+/-253	0	+/-16
Two or more races:	311	+/-129	0	+/-16
Two races including Some other race	179	+/-85	0	+/-16
Two races excluding Some other race, and three or more races	132	+/-106	0	+/-16

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

While the 2013-2017 American Community Survey (ACS) data generally reflect the February 2013 Office of Management and Budget (OMB) definitions of metropolitan and micropolitan statistical areas; in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB definitions due to differences in the effective dates of the geographic entities.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2013-2017 American Community Survey 5-Year Estimates

Explanation of Symbols:

1. An '***' entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
2. An '-' entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution.
3. An '-' following a median estimate means the median falls in the lowest interval of an open-ended distribution.
4. An '+' following a median estimate means the median falls in the upper interval of an open-ended distribution.
5. An '****' entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
6. An '*****' entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
7. An 'N' entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
8. An '(X)' means that the estimate is not applicable or not available.

B17001

POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE

Universe: Population for whom poverty status is determined
2013-2017 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.

	Delaware County, Indiana		Census Tract 24.02, Delaware County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error
Total:	108,823	+/-579	8,687	+/-309
Income in the past 12 months below poverty level:	23,018	+/-1,234	496	+/-224
Male:	10,021	+/-732	168	+/-104
Under 5 years	966	+/-164	0	+/-16
5 years	103	+/-67	0	+/-16
6 to 11 years	700	+/-170	0	+/-16
12 to 14 years	532	+/-145	0	+/-16
15 years	152	+/-74	0	+/-16
16 and 17 years	278	+/-92	0	+/-16
18 to 24 years	3,489	+/-491	74	+/-80
25 to 34 years	1,034	+/-247	13	+/-21
35 to 44 years	745	+/-200	0	+/-16
45 to 54 years	884	+/-187	11	+/-24
55 to 64 years	637	+/-146	58	+/-52
65 to 74 years	342	+/-113	12	+/-25
75 years and over	159	+/-63	0	+/-16
Female:	12,997	+/-784	328	+/-184
Under 5 years	920	+/-176	14	+/-22
5 years	111	+/-67	0	+/-16
6 to 11 years	959	+/-197	0	+/-16
12 to 14 years	321	+/-118	0	+/-16
15 years	102	+/-60	0	+/-16
16 and 17 years	248	+/-95	0	+/-16
18 to 24 years	4,746	+/-439	75	+/-118
25 to 34 years	1,646	+/-238	16	+/-25
35 to 44 years	1,131	+/-205	13	+/-26
45 to 54 years	895	+/-160	77	+/-58
55 to 64 years	967	+/-171	91	+/-91
65 to 74 years	504	+/-141	14	+/-22
75 years and over	447	+/-139	28	+/-30
Income in the past 12 months at or above poverty level:	85,805	+/-1,271	8,191	+/-370
Male:	43,222	+/-734	4,185	+/-300
Under 5 years	1,875	+/-168	159	+/-81

	Delaware County, Indiana		Census Tract 24.02, Delaware County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error
5 years	509	+/-193	16	+/-26
6 to 11 years	2,573	+/-251	331	+/-141
12 to 14 years	1,582	+/-291	159	+/-102
15 years	605	+/-167	123	+/-70
16 and 17 years	1,034	+/-134	198	+/-88
18 to 24 years	5,400	+/-459	379	+/-151
25 to 34 years	5,400	+/-229	469	+/-156
35 to 44 years	5,084	+/-220	552	+/-103
45 to 54 years	5,868	+/-210	674	+/-125
55 to 64 years	5,887	+/-159	437	+/-123
65 to 74 years	4,368	+/-138	432	+/-118
75 years and over	3,037	+/-103	256	+/-88
Female:	42,583	+/-863	4,006	+/-282
Under 5 years	1,885	+/-182	204	+/-69
5 years	665	+/-181	56	+/-47
6 to 11 years	2,557	+/-322	300	+/-121
12 to 14 years	1,190	+/-241	181	+/-103
15 years	464	+/-142	52	+/-44
16 and 17 years	1,101	+/-146	95	+/-64
18 to 24 years	3,723	+/-459	155	+/-90
25 to 34 years	4,727	+/-227	482	+/-160
35 to 44 years	4,914	+/-240	553	+/-113
45 to 54 years	6,240	+/-201	634	+/-147
55 to 64 years	6,099	+/-188	491	+/-123
65 to 74 years	5,155	+/-140	421	+/-113
75 years and over	3,863	+/-201	382	+/-111

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

While the 2013-2017 American Community Survey (ACS) data generally reflect the February 2013 Office of Management and Budget (OMB) definitions of metropolitan and micropolitan statistical areas; in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB definitions due to differences in the effective dates of the geographic entities.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2013-2017 American Community Survey 5-Year Estimates

Explanation of Symbols:

1. An '***' entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
2. An '-' entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution.
3. An '-' following a median estimate means the median falls in the lowest interval of an open-ended distribution.
4. An '+' following a median estimate means the median falls in the upper interval of an open-ended distribution.
5. An '****' entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
6. An '*****' entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
7. An 'N' entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
8. An '(X)' means that the estimate is not applicable or not available.

Categorical Exclusion

Appendix J

Other Information

Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated December 2019)

ProjectNumber	SubProjectCode	County	Property
1800234	1800234	Delaware	White River Park
1800244	1800244	Delaware	Daleville County Park
1800246	1800246	Delaware	Gaston Town Park

Please note, some of the property names are cut off on the ends due to character limits
 Also, park names may have changed and is not reflected on the list.
 *Various - this may include multiple sites in multiple counties and should always be included in your searches by county.

Bridge Inspection Report

18-00125
NEBO ROAD
over
YORK PRAIRIE CREEK



Inspection Date: 11/09/2018

Inspected By: James R. Lesh

Inspection Type(s): Routine

Inspector: James R. Lesh
Inspection Date: 11/09/2018

Asset Name: 18-00125
Facility Carried: NEBO ROAD

Bridge Inspection Report



PHOTO 1

Description LOOKING SOUTH FROM ROAD



PHOTO 2

Description LOOKING NORTH FROM ROAD

Inspector: James R. Lesh
Inspection Date: 11/09/2018

Asset Name: 18-00125
Facility Carried: NEBO ROAD

Bridge Inspection Report



PHOTO 3

Description LOOKING EAST FROM CHANNEL



PHOTO 4

Description LOOKING WEST FROM CHANNEL

Inspector: James R. Lesh
Inspection Date: 11/09/2018

Asset Name: 18-00125
Facility Carried: NEBO ROAD

Bridge Inspection Report



PHOTO 5

Description GAP IN WIDENING EAST COPING



PHOTO 6

Description GAP IN WIDENING WEST COPING

Inspector: James R. Lesh
Inspection Date: 11/09/2018

Asset Name: 18-00125
Facility Carried: NEBO ROAD

Bridge Inspection Report



PHOTO 7

Description DIAGONAL CRACKING IN DECK NORTHWEST CORNER



PHOTO 8

Description EROSION NORTHWEST CORNER

Inspector: James R. Lesh
Inspection Date: 11/09/2018

Asset Name: 18-00125
Facility Carried: NEBO ROAD

Bridge Inspection Report



PHOTO 9

Description WEST WIDENING SEPARATION FROM SLAB



PHOTO 10

Description SPALL AT BEAM BEARING NORTH PIER EAST COPING

Inspector: James R. Lesh
Inspection Date: 11/09/2018

Asset Name: 18-00125
Facility Carried: NEBO ROAD

Bridge Inspection Report



PHOTO 11

Description TYPICAL STEEL BEAM DETERIORATION



PHOTO 12

Description SCALING AT NORTH PIER NORTH PIER EAST SIDE

Inspector: James R. Lesh
Inspection Date: 11/09/2018

Asset Name: 18-00125
Facility Carried: NEBO ROAD

Bridge Inspection Report



PHOTO 13

Description UNDERCUT SLOPE WALL SOUTH BENT EAST SIDE



PHOTO 14

Description SPALL BEAM BEARING SOUTH PIER EAST COPING

Inspector: James R. Lesh
Inspection Date: 11/09/2018

Asset Name: 18-00125
Facility Carried: NEBO ROAD

Bridge Inspection Report



PHOTO 15

Description SPALL AT BEAM BEARING SOUTH PIER WEST COPING